



**LEDUC
COUNTY**

What We Heard Report

Transportation Master Plan Phase one: public participation

Date: May 4, 2021

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Background

Transportation Master Plan

Leduc County is updating its Transportation Master Plan (TMP), which will guide short, medium and long-term transportation infrastructure investment and establish our vision for transportation in Leduc County over the next 20 years.

Transportation in Leduc County includes many different forms of movement for a wide-range of residents, businesses and industries. The TMP will take a location-based approach to evaluate and plan for each of the unique areas throughout the county to develop an integrated multi-modal transportation system where people can drive, walk, bike and ride transit efficiently, safely and conveniently.

The TMP will include an assessment of our current transportation system, and the regional growth nodes, to develop comprehensive infrastructure plans to address the diverse needs of the urban, rural, business, industrial, agricultural, recreational and lakeshore communities.

The TMP will guide Leduc County using a two-phased approach:

- 1) **Transportation Master Plan:** Leduc County's Transportation Master Plan is the framework that guides transportation-related decision making. The TMP will consider all types of movement in order to design an interconnected, multi-modal transportation system where people can drive, walk, bike and ride transit efficiently and conveniently to their desired location. It is a broad, long-term view of the county's transportation network, highlighting important transportation corridors within the county and region to facilitate industrial and residential growth and strive for continued safe transportation routes.
- 2) **Affordability/Implementation Plan:** This part of the final plan will reflect the strategic objectives of Leduc County, provide direction for sound roadway investments and guide efficient transportation within the county for the upcoming five-and-10-year capital plans.

Public participation

Pre-engagement

In Nov. 2020, pre-engagement Interviews and surveys were conducted with a sample of individuals representing firms, organizations, community associations, hamlets and adjacent municipalities. Pre-engagement was completed to gather feedback on what each individual perceived as meaningful engagement and to provide insight into the variety of interests and issues concerning transportation services offered within the county.

The information collected during the pre-engagement period was used to help guide the communication process, provide a vision related to the required information materials and serve to identify considerations and issues concerning engagement planning.

As part of the pre-engagement process, we conducted the following:

- Six interviews were conducted with representatives from key audiences, including:
 - Rolly View Community Association
 - Glen Park Community Association
 - Agricultural grain producer from west Leduc County
 - Edmonton International Airport
 - Leduc Chamber of Commerce
 - City of Leduc's director of engineering
- 23 online surveys were completed by various audiences*:
 - residents
 - community organizations
 - businesses
 - agricultural sector
 - surrounding municipalities
 - municipalities within Leduc County
 - members of administration (Agriculture Services, Engineering, Road Operations, Parks and Recreation, Economic Development and Fire Services)

**See Appendix A for a complete list of those who were invited to participate in the survey.*

- A Supporting Success workshop was held to highlight TMP planning undertaken to date, share the engagement approach, confirm input required to inform project decision making and commitment to consider public input, and explore roles in the successful implementation of the engagement process. The workshop included the following members:
 - Leduc County council
 - Leduc County Executive Leadership Team
 - Directors of Planning and Development, Engineering and Utilities and Agriculture Services and Road Operations

Public participation

The Leduc County TMP will affect many different audiences, including the general public, surrounding municipalities, the province (Alberta Transportation) and Leduc County staff. It's important to Leduc County to hear from and work with these audiences throughout the TMP process to gather meaningful feedback.

For phase one public participation, we worked with the public to understand the issues, concerns and causes of those concerns within the existing transportation network and to identify areas and ideas for improvements, enhancements and changes. The input received will be reflected in the analysis of our current transportation challenges, needs and solutions. For this reason, **IAP2 spectrum level: involve** was selected for this project.

We launched a month-long engagement plan in February that utilized an online engagement platform called Social Pinpoint, which allowed our audiences to provide their input using an interactive mapping tool and online surveys. We also hosted two virtual workshops on Feb. 18 and Feb. 22.

What we asked of you

Throughout the month of February, participants were asked for input on the following:

- Current modes of transportation they utilize
- Top three locations they travel to on a weekly basis
- Most and least critical transportation issues in Leduc County
- Input on the draft TMP vision statement and goals

By using our interactive map of Leduc County, participants were also asked to identify what's working, what's not working and ideas for improvement.

How we communicated

HOW WE COMMUNICATED

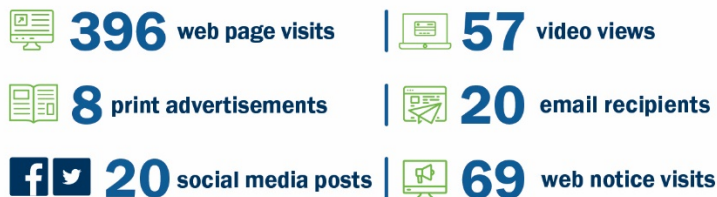


Figure one: How we communicated

Advertisements

We undertook an extensive advertising campaign that included:

- 12 roadside signs placed throughout the county
- Utility bill inserts
- Paid and organic social media ads
- A webpage created for the TMP, www.leduc-county.com/transportation-planning
- Two 30-second videos
- A web notice on the Leduc County website
- Direct mailed letters to stakeholders

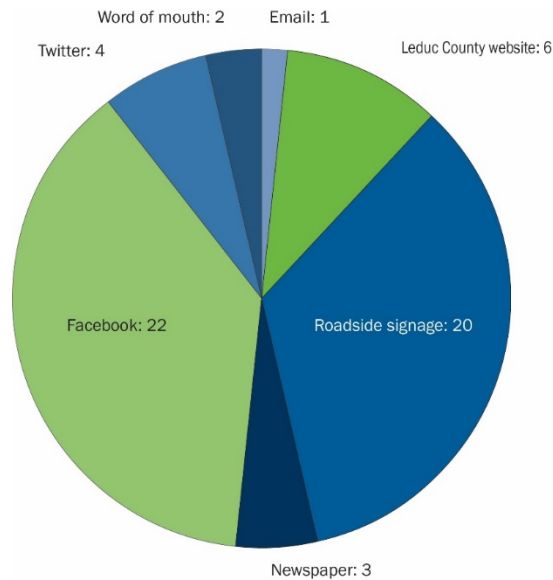


Figure two: How did you hear about the Leduc County Transportation Master Plan public participation opportunity?

Methods of Engagement

HOW YOU PARTICIPATED



52 survey respondents



108 map comments



32 social media comments



795 Social Pinpoint visits

Figure three: How you participated

Online, live facilitated workshops

Using the Zoom platform, these open public sessions allowed participants to join online from their homes or offices, learn about what's being considered in developing the TMP, ask questions and provide their feedback. Two choices of dates and times were offered to accommodate availability, both which covered the same content.

Online, Static Engagement

Online engagement was initiated for individuals to contribute their ideas whenever and wherever it was most convenient for them. The Social Pinpoint platform was utilized for static engagement, and included:

- A landing page to post all resources, materials and events related to the TMP and the engagement process.

- A mapping tool where issues and ideas were able to be pinned to a map of Leduc County for ease of geographical reference. This tool contained a variety of information on Leduc County roadways, Alberta Transportation roadways, background information for the Leduc Transportation Master Plan and access to surveys.
- Built-in survey capabilities where questions were embedded within the mapping tools and included as stand-alone survey for the draft vision statement and TMP objectives.

Engagement by numbers: Social Pinpoint statistics

The Leduc County TMP team measured the following Social Pinpoint statistics throughout the four-week, phase one public participation period:

- More than 3,000 Social Pinpoint site visits from almost 800 unique individuals.
- 75 individuals that provided 108 comments on the project map.
- More than 45 comments on Leduc County Facebook posts in the months of January and February.
- More than 50 survey responses to the Issues and Concerns survey and TMP Vision and Goals survey.

The team also hosted two zoom sessions which had an attendance of 10 participants.

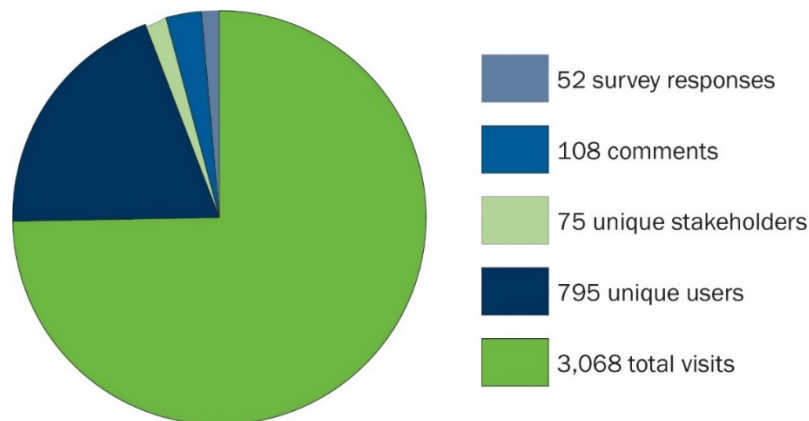


Figure four: Social Pinpoint engagement metrics, Leduc County Transportation Master Plan, phase one public participation

What you told us

The following sections provide a summary of the comments and survey responses received through the online survey and the online mapping tool. It also summarizes the key concerns we heard during the two public Zoom sessions.

The received comments, concerns, questions and survey responses are from an open information location, and are not a statistically random sample of all Leduc County residents. Due to the nature of the surveys and responses, the results are a collection of opinions and perceptions from interested or potentially affected residents, are qualitative in nature and cannot be said to represent all views of Leduc County citizens.

Draft TMP Vision

This survey asked participants to rate their support and provide an open-ended comment on the following statement:

"High quality of life and regional economic growth are achieved through the Leduc County integrated multi-modal transportation network, emboldening sustainable travel choices while supporting residents and industries to move with confidence."

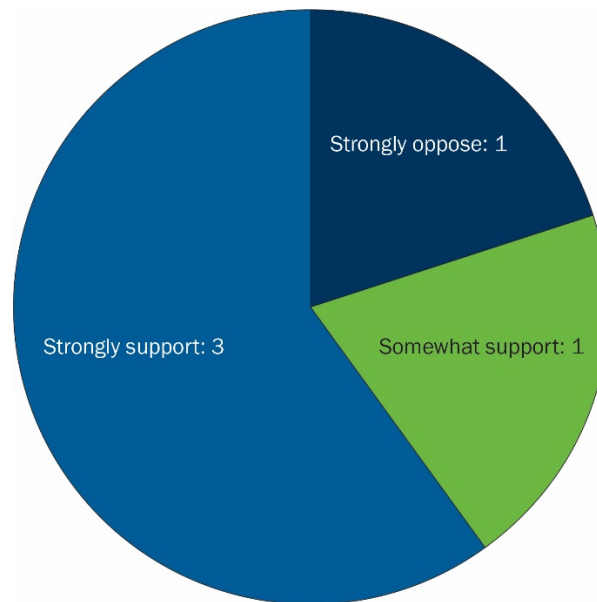


Figure five: Leduc County Transportation Master Plan draft vision statement, "Indicate your level of support for the draft vision"

Key takeaways

80 per cent of respondents indicated positive support for the draft vision statement. The primary themes garnered from responses to the draft vision statement are concerns of safety, sustainability and active transportation. Some of the comments received include the following:

- "As a Leduc County resident and business owner, I support this statement. "
- "Confidence is not something I consider when driving roads. A roadway system needs to be sustainable and continually maintained to ensure a proper level of service. Micro surfacing, crack sealing etc. A road quality index should be maintained."
- "Sustainable travel choices while supporting residents and business move forward"
- "I agree with it but I think there is work to get there. My main concern is the safety of our roads. The area continues to become busier and improvements are needed for the safety of drivers. I also think that activities like walking and biking are becoming more common but many of the roads are not safe or supportive of these things."

Goals and objectives

We asked respondents to rank the following goals and provide an open-ended statement regarding their opinion on what could be included.

Goal 1: Meet the transportation infrastructure needs of the present and future urban, rural, agricultural, industrial, resource, business and lakeshore communities.

Goal 2: Identify and recommend infrastructure investment and implementation that supports sustainable and integrated multi-modal transportation that delivers fiscal responsibility, common-sense investment and supports the needs of Leduc County as a whole for the next two decades.

Goal 3: Create a vision in Leduc County that enhances roadway safety, accessibility, equity and inclusivity; that supports active, healthy and livable communities.

Goal 4: Achieve and Support the policy directions of the County's MDP and Strategic Plan.

Goal 5: Undertake a comprehensive public engagement process to seek ideas and input from the broadest possible sample of residents, businesses and industries.

Some of the comments we received include the following:

- *"I would prefer the improvements of the systems we have more of a priority than the addition of new transportation options. Thinking of the future needs is also important as short-term fixes have been exactly that – short-term and probably more expensive in the long term."*
- *"Need a walking/biking trail to Beaumont. I believe this would be frequently used. Would allow the community to be more active and access Beaumont without needing to drive your vehicle. Would be a step to reduce carbon footprint and improve quality of life."*

We also heard that goals related to active transportation should be considered in the Leduc County Transportation Master Plan.

Survey: key transportation issues and concerns

We received 48 responses to our key transportation issues and concerns survey in the phase one public participation period. This survey was intended to gather general feedback on how citizens travel in Leduc County and what the key transportation issues are.

Key takeaways

- The primary travel mode is by personal vehicle (100 per cent of respondents), while five per cent of respondents also travel by heavy truck and by transit.
- Heavy truck traffic was ranked as the highest transportation concern, as 90 per cent of respondents ranked this issue as their top issue (see exhibit four below).

- Active transportation opportunities were ranked as the second key concern for Leduc County transportation, with more than 50 per cent of respondents having ranked this issue in their top two concerns (see exhibit four below).

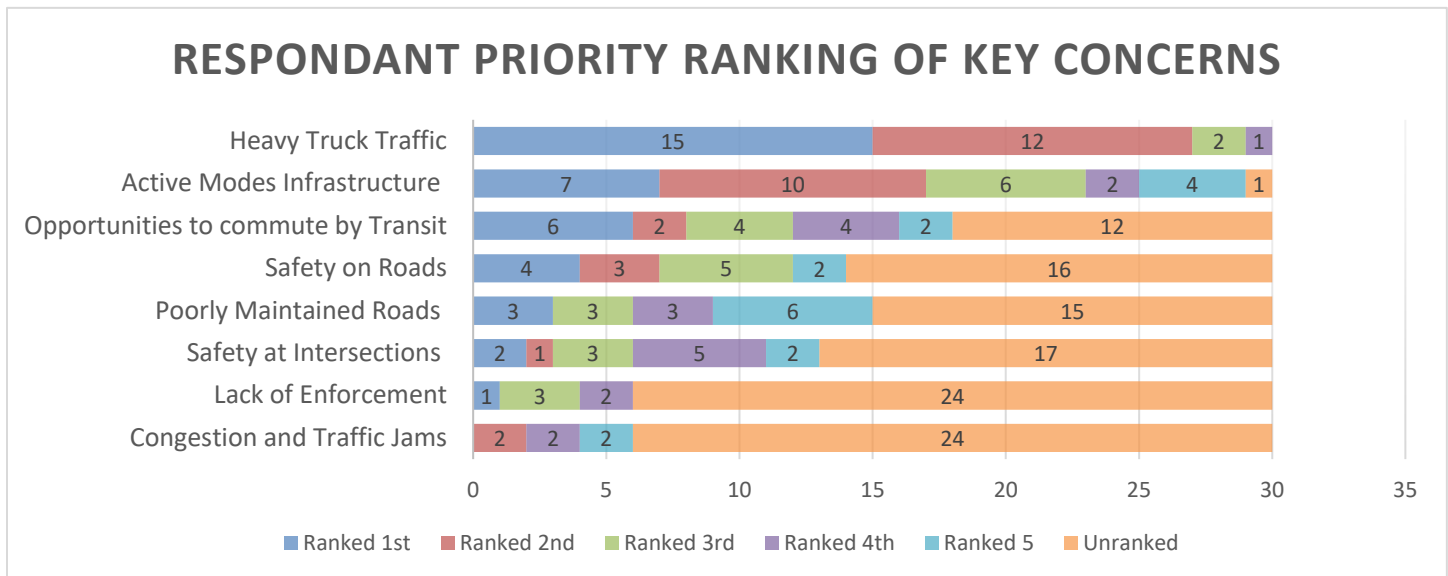


Figure six: Summary of the ranking of key concerns; the lower the rank indicates the greater priority

In addition to the transportation issues identified by the study team, respondents indicated that roadway maintenance, paving and general improvements to county roads to reduce wear and tear on vehicles were important issues to be considered in the Leduc County TMP. Some of the comments we received include the following:

- “Buford highway is full of potholes and frost cracks, and is poorly maintained with road emulsion tar and rock chips. This mile and a quarter section need proper repair and resurfacing.
- The intersection at RR 275 & TWP 494 at the hamlet itself, the road has a huge sunken frost heave across both lanes that is a safety concern if someone loses control and crashes into adjacent properties, homes or pedestrians.”

Social Pinpoint map comments

The Social Pinpoint mapping tool (see figure 11) was used to gather public comments by geolocation and provide an opportunity for the public to engage in discussions stemming from these comments. Comments were separated into three categories:

1. Ideas and suggestions: 29 comments received
2. What’s not working: 77 comments received
3. What’s working: five comments received

A variety of comments related to traffic operations along Highway 2, Highway 2A, Highway 19, Highway 60 and Highway 623 – all roads under the Alberta Transportation’s jurisdiction – were received. The Leduc County TMP update will consider these comments for future discussions with the province.

The Leduc County-related comments were reviewed and divided into the following key themes:

Maintenance issues

- Existing roads suffer from annual free-thaw cycles and annual flooding, resulting in poor travel conditions that impact residents, delivery vehicles, agricultural vehicles and emergency services. The quality and quantity of paving is viewed as insufficient and should be improved at several locations.

Roadway operation concerns

- These types of concerns were related to the speed of roadways, the need for intersection turning lanes, the need for intersection traffic signals and the general operation of major corridors. Examples include:
 - Township Road 510 was frequently identified as a narrow road with poor shoulders, high traffic volumes and shallow ditches with a need to be widened.
 - Several rail crossings (namely, the Township Road 504 railway crossing) were also considered under poor condition.
 - Finally, concerns were raised due to desired change in the posted speed limits along Glen Park Road and 9 Street.

Transportation planning needs

- This theme involved comments regarding future plans for new roadways, new active transportation opportunities and provisions for the movement of goods. These comments related to the future extension of the Nisku Spine Road along 9 Street, the concern that heavy trucks by-pass weigh scales to utilize Glen Park Road, and the long-term function of Township Road 500 nearest the future Northwest Saunders Lake ASP.

Intersection safety concerns

- Respondents identified key intersection safety concerns, such as sight lines, busy intersections, and the prevalence of illegal maneuvers that could increase collision rates. Some of the identified locations include:
 - Highway 2A / Highway 623 (Alberta Transportation jurisdiction)
 - Glen Park Road and Range Road 263
 - The intersection of Highway 2A / Glen Park Road and Highway 623/Range Road 263
 - Sample comment:

- *“Really really hate the Highway 2A and Glen Park road intersection!! People pass in the turn lanes all the time making it extremely dangerous to make a left turn. We often make a turn with a horse trailer and people will pass us on the right hand side and they cannot be seen beside the trailer while passing. Either a set of lights or stepping up enforcement to start nailing those who pass illegally both at that intersection and along highway 2A. Highway 2A also needs to be twinned asap!”*

Active transportation

- Several comments requested additional opportunities for recreational activities, such as walking and cycling options throughout Leduc County. This theme is well supported from the primary survey that identified active transportation as a key issue for the Leduc County TMP to address.

Who engaged

The Leduc County phase one public participation process reached a wide variety of audiences within and surrounding the county. This included:

- More than 50 per cent of participants were between the ages of 34 and 50
- More than 85 per cent of participants live and pay taxes to Leduc County;
- More than 30 per cent of participants learned something about Leduc County transportation
- More than 40 per cent believed they are well informed.
- About 30 per cent of participants were in agreement that Leduc County is listening, understanding and committed to considering public input in the Leduc County TMP process.



Figure seven: Age demographics of participants

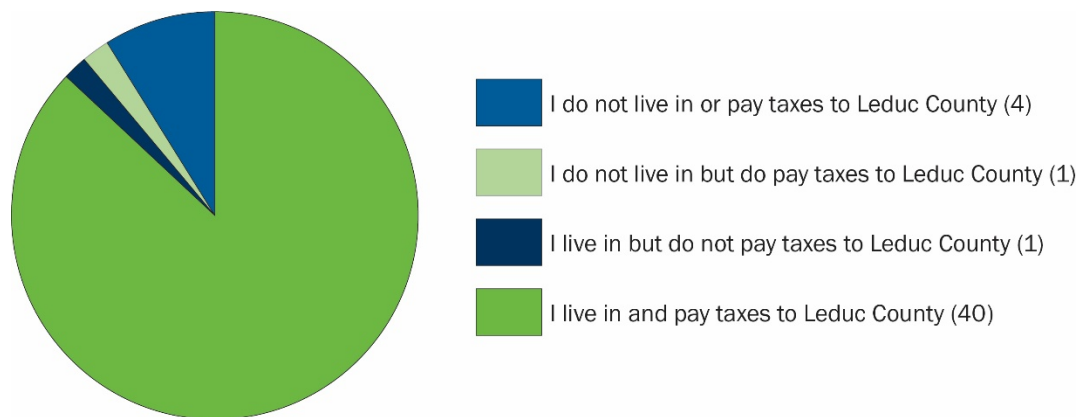


Figure eight: Location demographics

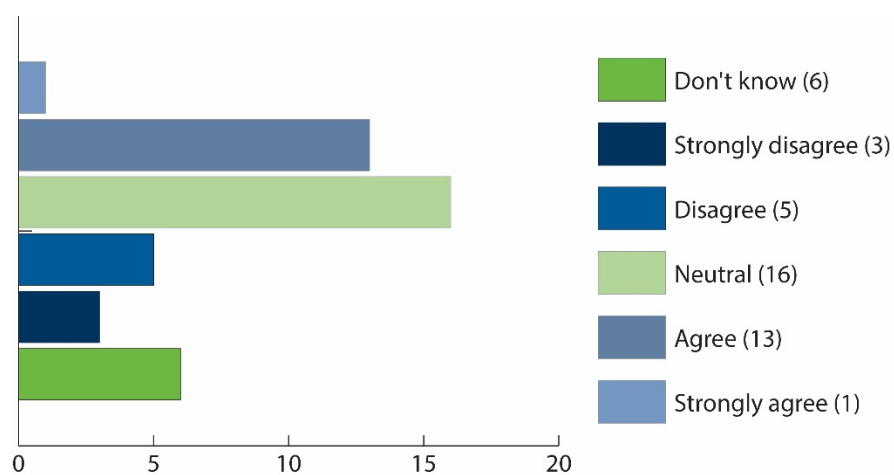


Figure nine: Leduc County will consider my input

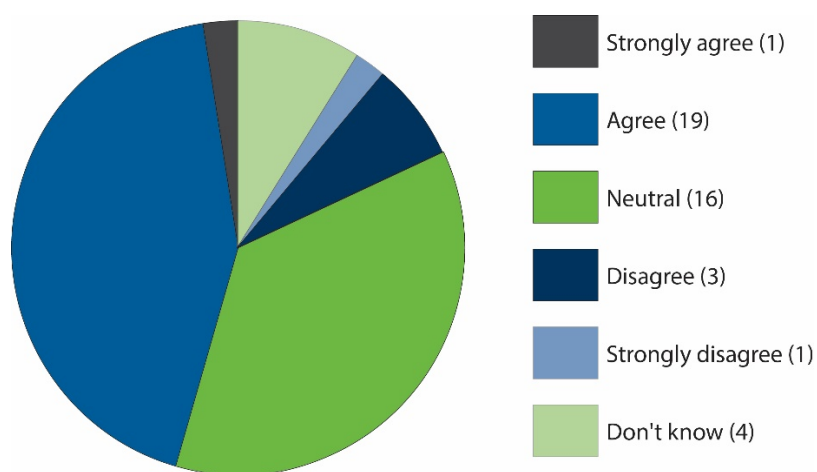


Figure 10: Was the right information provided?

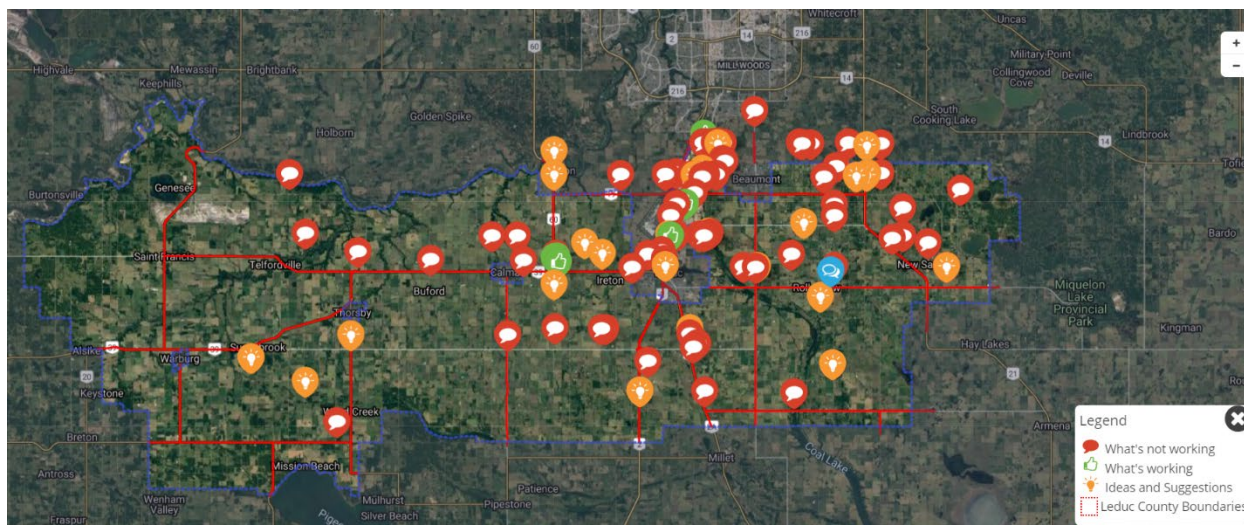


Figure 11: Social Pinpoint map

Municipal and key stakeholder engagement

Phase one of the TMP public participation process also included an extensive outreach to adjacent municipal partners, key industry stakeholders and major businesses that would be impacted. We held meetings to inform them of the Leduc County TMP update, collect their feedback and to collaborate on future transportation issues.

Meetings were held with representatives from:

- Edmonton Metropolitan Region Board (EMRB)
- Alberta Transportation, north-central region
- City of Leduc
- City of Beaumont
- Parkland County
- Strathcona County
- County of Wetaskiwin
- County of Camrose
- Brazeau County
- Town of Thorsby
- Town of Devon
- Town of Calmar
- Village of Warburg
- Edmonton International Airport
- Capital Power – Genesee Generating Station

The stakeholder engagement process informed the Leduc County TMP on many key issues, including:

- The long-term vision and planning requirements for Township Road 510.
- The desire to connect Leduc County, the City of Beaumont, the City of Edmonton and the City of Leduc through recreation trails.
- Investigating a walking trail partnership and community/senior van agreement with the Village of Warburg.
- Discussions with Alberta Transportation regarding future improvements along Highway 60, Highway 19, Highway 39 and Highway 21.
- Discussing the future needs of the Nisku Spine Road corridor as design and construction proceeds to the south.

We will continue to engage with key stakeholders throughout the TMP process, as their input will help provide a well-informed plan.

Next steps

The next phase of the Leduc County Transportation Master Plan will evaluate the County transportation system assuming the land uses and policy framework established from within the Leduc County Municipal Development Plan (MDP). This evaluation will be used to develop a variety of transportation network improvement options and transportation-related policies to better improve Leduc County's auto, cycling, pedestrian and transit network. The evaluation will strongly consider and incorporate the public information obtained to date, which will help develop a plan that supports the betterment of Leduc County's transportation system.

Phase two of public participation will involve the presentation of draft recommendations, policies and strategies proposed in the draft Transportation Master Plan. Phase two will take place in July 2021.

For more information on the Transportation Master Plan project, please contact Khushnud Yousafzai at 780-955-4590.

APPENDIX A: Survey Invitations to Participate to Develop the Communications and Engagement Plan

Rosenau Transport
Ascent Transport
Latium - Fleet & Asset Management
Copper Tip Energy Service
Freudenberg Oil & Gas Canada
Drilling Tools International
Argus Machine Co.
Alta-Fab Structures Ltd.
Advanced Panel Products
Bison Producers of Alberta
Ace Vegetation Service Ltd.
2020 Seed Labs Inc.
Gruger Family Fungi
Dynaleo
Rig Hand Distillery
Edmonton Metropolitan Region Board (EMRB)
Alberta Transportation
City of Leduc
City of Edmonton
Parkland County
Strathcona County
County of Wetaskiwin
County of Camrose
Town of Thorsby
Town of Devon
Town of Calmar
Town of Calmar
Village of Warburg
City of Beaumont
Brazeau County
CN Rail
Canadian Pacific Railway (CP Rail)
Wizard Lake Watershed and Lake Stewardship Association
Pigeon Lake Watershed Association
Beaumont & District Agricultural Society
Centre Lodge Community Centre
Clover Lawn Community Center Association
East West Millet Rural Fire Department Society
Genesee Agricultural Society
Glen Park Community Association
Glen Park Cemetery Maintenance Society

Canadian Energy Museum
Leduc West Antique Society
Looma Good Deed Association
Morrowdale Community Centre
New Humble Centre Parents Group
New Sarepta Volunteer Firefighters Association
Nisku Volunteer Firefighters Association
Rolly View Community Association
Rundle's Mission Society
Sandholm Pigeon Lake Drifters
Strawberry District Society
Sunnybrook Athletic Association
Telford Community Club
Telfordville Community Centre
Thorsby & District Fish & Game Association
Thorsby Agricultural Society
Willow Creek Community Association
Wilton Park Community Centre