



Bylaw No. 12-22
Schedule A

East Vistas Area Structure Plan

Sections 29 and 32-50-24-4 and part of NE 31-50-24-4

Approved by Bylaw 15-09

Consolidated with amending bylaws 28-15, 12-17, 23-17 and 12-22

Version: May 10, 2022



Note

Anyone using this consolidation is advised that the amendments have been included for convenience only and that the original area structure plan and the original amending bylaw should be consulted for all purposes of interpreting and applying the bylaw.

BYLAW NO. 15-09

LEDUC COUNTY

A BYLAW OF LEDUC COUNTY, IN THE PROVINCE OF ALBERTA, TO ADOPT THE EAST VISTAS LOCAL AREA STRUCTURE PLAN

WHEREAS

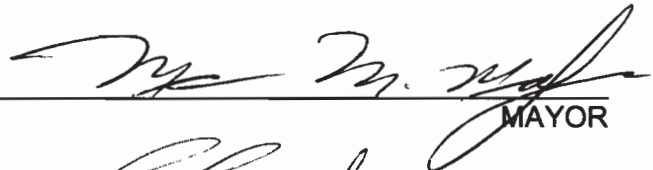
the Council of Leduc County deems it to be in the public interest to adopt the East Vistas Local Area Structure Plan;

NOW THEREFORE, THE COUNCIL OF LEDUC COUNTY DULY ASSEMBLED ENACTS AS FOLLOWS:

that the East Vistas Local Area Structure Plan, being Schedule 'A' attached to and forming part of this Bylaw, be adopted.

This Bylaw shall take effect on the date of the third reading

Read a first time this 8th day of September, A.D., 2009.


MAYOR

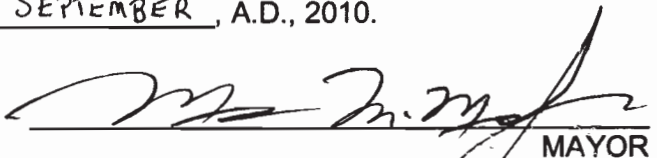

ACTING COUNTY MANAGER

Read a second time this 22nd day of July, A.D., 2010.


MAYOR


COUNTY MANAGER

Read a third time and passed this 28 day of SEPTEMBER, A.D., 2010.


MAYOR


COUNTY MANAGER

Amendments to the East Vistas Area Structure Plan

Office consolidation

Bylaw number		Date	Amendment
Bylaw 28-15		July 7, 2015	Re: Municipal reserves (Section 6.5)
Bylaw 12-17		May 23, 2017	Figure 6 - Development concept, tables 1 and 2
Bylaw 23-17		August 22, 2017	Figure 6 - Development concept, tables 1 and 2
Bylaw 12-22		May 10, 2022	<ul style="list-style-type: none">- Title amended- Removed 'local' from every occurrence of plan title- Removed references to North Major ASP- Amended reference from 'Town of Beaumont' to 'City of Beaumont'- Formatting of document

Note

Anyone using this consolidation is advised that the amendments have been included for convenience only and that the original area structure plan and the original amending bylaw should be consulted for all purposes of interpreting and applying the bylaw.

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1 Introduction

1.1 Preface

In response to demand for land to accommodate growth in the greater Capital Region, proponents of the East Vistas Area Structure Plan propose to develop a sustainable urban community in Leduc County. The area structure plan includes the lands adjacent to the existing country residential subdivisions known as The Vistas. It encompasses an area of approximately 562 ha (1,400 acres) that includes all of Sections 29 and 32 50-24-W4 and a portion of the NE 31 50-24-W4 southeast of Irvine Creek.

The plan area is serviced with municipal water and sanitary sewer. It will be developed to an urban standard, with paved roads, curbs, gutters, underground utilities and sidewalks.

The boundary of the City of Edmonton is approximately three kilometres to the north; the northern boundary of the City of Leduc is three kilometres to the south; and the City of Beaumont is just under two kilometres to the east. To the west lie The Vistas, Blackmud Creek, and beyond that Nisku Industrial Park. Township Road 510 is the north boundary of the plan area, Range Road 244 is the east boundary, and Highway 625 forms the south boundary and provides access to the Edmonton International Airport and Queen Elizabeth II Highway (both approximately five kilometres west). The location and plan area are shown in Figure 1.

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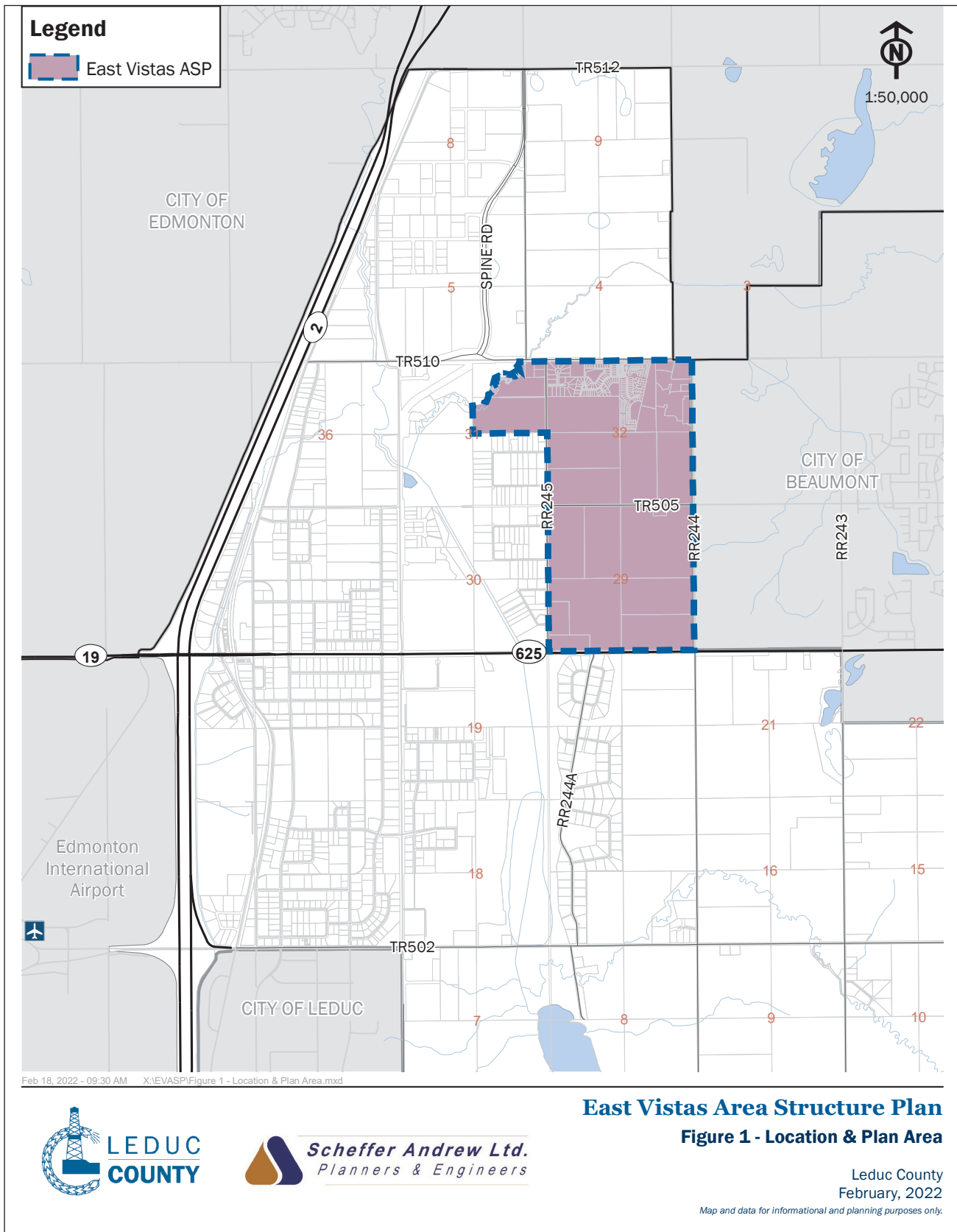
1.2 Purpose

The East Vistas Area Structure Plan provides an overview of the land use concept and describes the area, services, transportation, and servicing requirements needed to support the development. The plan shows how the development will integrate with the existing and future land uses in the vicinity. When adopted by Leduc County, it will provide guidance for the future redistricting and subdivision applications within the plan area, both in terms of organization and design.

1.3 Planning Context

The planning authority is Leduc County. The approval of this area structure plan is under the jurisdiction of Leduc County, applicable regulatory agencies and the Edmonton Metropolitan Region Board, formerly known as the Capital Region Board.

Figure 1 - Location and plan area



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2 Vision and Goals

The Edmonton Capital Region includes the City of Edmonton, City of Leduc, City of Beaumont, and Leduc County, as well as twenty one other municipalities. This region has recently experienced one the most intense growth periods in its history. The unprecedented level of associated economic activity brought with it significant population increases, record construction activity, and major investments in the residential, commercial, industrial, transportation, and institutional sectors. Both the private and public sectors are now planning major investments in infrastructure to accommodate future growth. When coupled with the resultant demand for land to accommodate this growth, Leduc County will be faced with decisions on how to manage new growth within its boundaries, and particularly in the fringe areas adjacent to the Edmonton International Airport, Nisku Business Park, Leduc Business Park, City of Beaumont, City of Leduc, and the City of Edmonton.

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In response to these anticipated growth pressures, Leduc County has determined that some changes in land use, servicing requirements, and infrastructure planning are needed to accommodate growth in all sectors. The County is addressing its long-term planning, infrastructure, and growth management options through a strategic plan, new area structure plans, off-site levies, Inter-municipal Development Plans, partnership agreements, and regional initiatives. Similarly, a cooperative approach is needed to facilitate the planning for the East Vistas Area Structure Plan. This plan was prepared by Scheffer Andrew, engaged by the proponents of the East Vistas Area Structure Plan and Leduc County administration, using the terms of reference prepared by Leduc County.

A key feature of this area structure plan is that it will create a complete and sustainable urban community. The mix of land uses includes higher density residential lots than presently exist in the County. The goal of this area structure plan is to apply Smart Growth principles to encourage sustainability by accommodating growth that makes efficient use of the land and existing municipal services. Smaller single-family residential lots and multi-family sites will address the need for a wider range of housing choices. As well, healthy lifestyles will be promoted by including infrastructure for walking and cycling, as well as by making streets a welcoming public amenity for those not driving.

A centrally located Vistas Centre with a Main Street is located in the heart of the community. The wide sidewalks and continuous streetwall of buildings will make Main Street an inviting place for a casual stroll, a place to meet friends, or to shop at the various stores. Multiuse pathways will be integrated with stormwater ponds to inter-connect subdivisions with the community and the community with the Vistas Centre. Local commercial service areas will be connected by a recreational pathway system with open spaces that are intended to create a defined community with a strong Sense of Place. The pathway system will form a complete secondary circulation system and will accommodate people using a variety of modes of transport, from walking to cycling, in-line skating, skateboarding, pushing strollers, or using mobility aids.

Conservation practices included in the plan preserve environmentally sensitive areas, watercourses, and large existing woodlots. Residential lots will have landscaping standards to retain a sense of open space and celebrate the rural character and lifestyle that is so important to County residents. In addition, developments will be governed by design guidelines, architectural design controls, and landscaping guidelines. This is intended to give a sense of land use certainty to the residents, as well as ensuring that proper attention is paid to the public realm.

Figure 6 shows the distribution of the land uses and location of the recreational pathways, environmentally sensitive areas, collector roads, future school sites, as well as the commercial, mixed use and multi-family sites.

3 Plan Area Context

Leduc County recognizes its role in helping to define the Leduc sub-region of the Edmonton Capital Region. The sub-region is centred on the Highway 2 corridor and includes the cities of Leduc and Beaumont, Nisku, Edmonton International Airport, and the Town of Devon. The County has undertaken a comprehensive planning program with the aim of coordinating land use in the sub-region in conjunction with its regional neighbours. The results of these planning efforts are the Leduc County / Town of Devon Intermunicipal Development Plan (IDP), the City of Leduc / Leduc County IDP, the Highway 19 Area Structure Plan (ASP), the Leduc County / Town of Beaumont Joint Growth Study (JGS), and the East Vistas ASP. As shown in Figure 2, these project areas radiate outward from the centre of the sub-region and build upon earlier County planning in the Saunders Lake and Blackmud Creek ASPs. Taken together, the regional planning of which the East Vistas is a part provides policy direction to an area approximately sixteen miles from east to west, and some thirteen miles from north to south, encompassing parts of four municipalities and a major international airport.

3.1 Land Use

The current land use within the plan area is predominantly agriculture with some established farmsteads and smaller country residential parcels scattered throughout the site.

Agriculture is the predominant land use north and east of the plan area. One intensive livestock operation is operating on the NW 28-50-24-4, east of Township Road 510. The minimum setback requirement of 360 metres from the manure storage containment of this operation will not constrain development within the plan area.

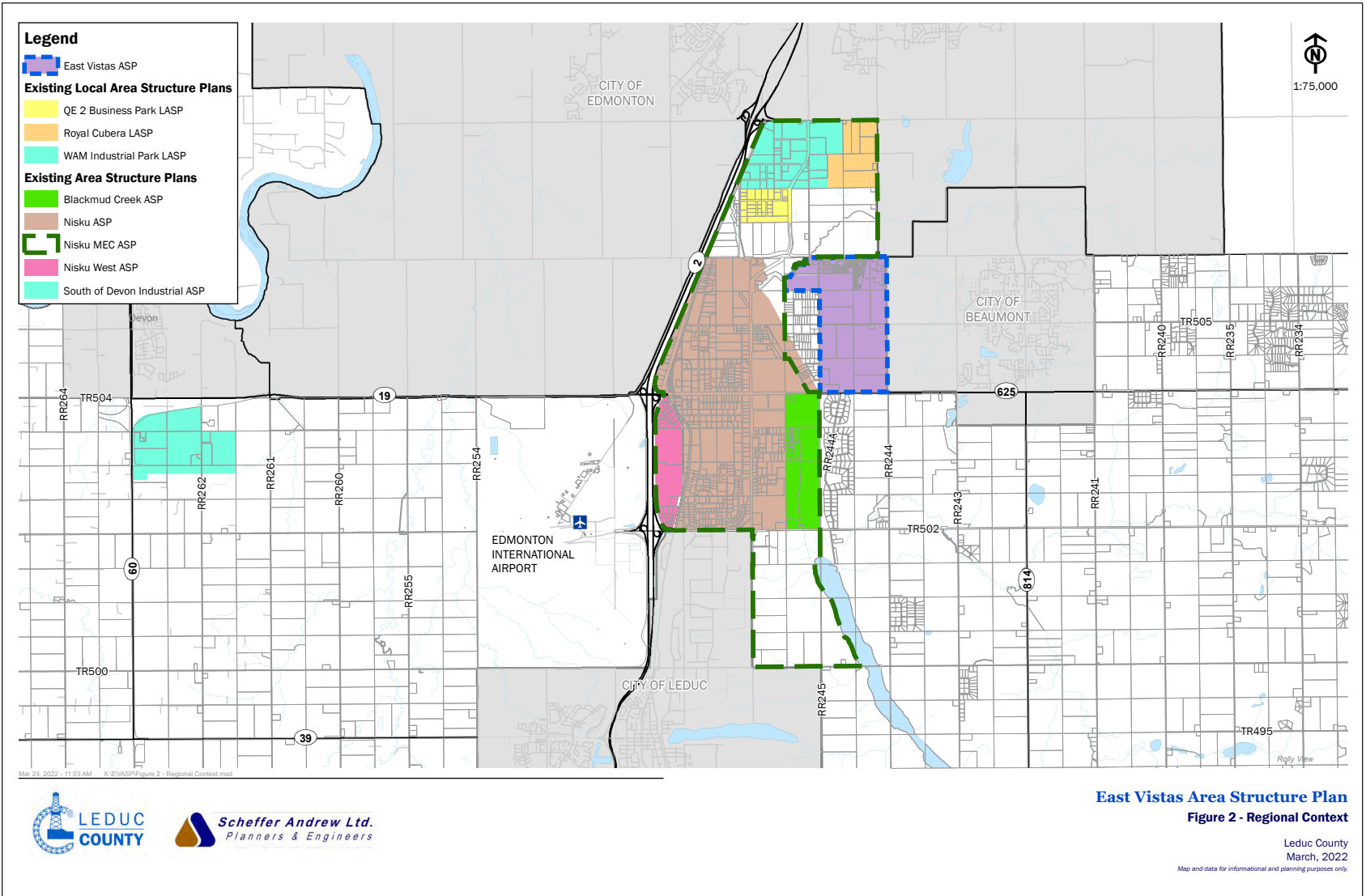
The land use west and south of the plan area is country residential. The established subdivisions are collectively known as “The Vistas”. An estate residential subdivision (Lukas Estates I) and a large lot urban development (Lukas Estates II) exist in the north half of the NW of Section 32. The East Vistas is contiguous to these developments.

Although lands in the plan area are considered high capability agricultural lands, there are compelling reasons to convert the area to non-agricultural land uses. The East Vistas will concentrate a significant amount of Leduc County’s future growth into a relatively small land area.

Major benefits may accrue to the County from higher intensity land development, such as achieving locally sustainable infrastructure, the concentration of community service delivery, a higher standard of living for area residents, the opportunity to develop a sub-regional hub that includes the City of Beaumont and possibly the City of Leduc, and the conservation of other high capability agricultural lands in the County and in the Capital Region as a whole. Finally, as municipal servicing is already available in the plan area, transportation facilities also service the area, and a major regional employment centre (Nisku Business Park) is essentially adjacent to the plan area, a higher intensity use of the land is warranted. Adjacent and existing land uses are shown in Figure 3.

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Figure 2 - Regional context



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3.1.1 Capital Region Growth Plan

The Capital Region Board released the Capital Region Growth Plan (CRGP) at the end of March, 2009, with two subsequent addenda released in October and December of the same year. That plan, which was accepted by the Minister of Municipal Affairs in March 2010, includes policies identifying where and how growth should occur. Section 2.2 of the Land Use Plan component of the Capital Region Growth Plan articulates six planning principles. Under principle II: Minimize Regional Footprint, part B: Concentrate New Growth Within Priority Growth Areas, a set of criteria are identified as follows:

- ii. *Priority shall be given to accommodating growth in major employment areas and in locations that meet at least three of the following four criteria:*
 - a. *Existing and proposed multi-use movement corridors, including transit nodes;*
 - b. *Adjacent to existing and proposed major employment areas;*
 - c. *Redevelopment and intensification opportunities within existing urban areas; and*
 - d. *Locations that utilize existing infrastructure and servicing capacity or logically and efficiently extend that infrastructure.*

The Capital Region Growth Plan defines Major Employment Areas as:

“Areas that have, or are anticipated to have, a significant concentration of jobs, such as Port Alberta, Acheson, Nisku, and Alberta’s Industrial Heartland.”

Often, land uses in the above named areas are incompatible with residential land uses. As a result, the practical application of policy B(ii) of the CRGP is that growth occurs ***in the vicinity*** of a major employment area such as Nisku. In the case of the East Vistas, Leduc County is proposing growth adjacent to Irvine Creek, which forms the eastern edge of Nisku. The plan thus satisfies the first part of policy B(ii).

As outlined above, the Capital Region Growth Plan states that “in addition to growth in major employment areas, priority shall be given to locations that meet three of the following four criteria.” The East Vistas responds to these four criteria in the following ways:

- a. Existing and proposed multi-mode movement corridors, including transit nodes. These are defined in the Capital Region Growth Plan as corridors “that accommodate multiple modes for the movement of people and goods within and between member municipalities in the Capital Region.” The functional planning study of Township Road 510 accommodates future transit, and this road connects the East Vistas to the City of Beaumont, a member municipality in the Capital Region. Furthermore, this ASP includes much language in support of multi-modal transport, including the incorporation of bicycle lanes in street cross sections in Appendix C, requirements for bicycle friendly paved multi-use pathways, new bicycle parking regulations proposed for the Leduc County Land Use Bylaw, and a town centre designed using transit supportive development principles. Finally, the East Vistas reflects Figure 1 Regional Transportation Infrastructure in the December 2009 Addendum of the Capital Region Growth Plan by identifying Highway 625 as a future regional intermunicipal transit route by designating a site as a regional intermunicipal transit stop and orienting adjacent land uses to enhance this stop. - Bylaw 12-22
- b. Adjacent to existing and proposed major employment areas. As outlined above, the ASP meets this criterion.
- c. Redevelopment and intensification opportunities within existing urban areas. This criterion depends upon the definition of ‘urban’. If the definition requires the area to be municipally serviced and have access to high order transportation, then the East Vistas meets this criterion.

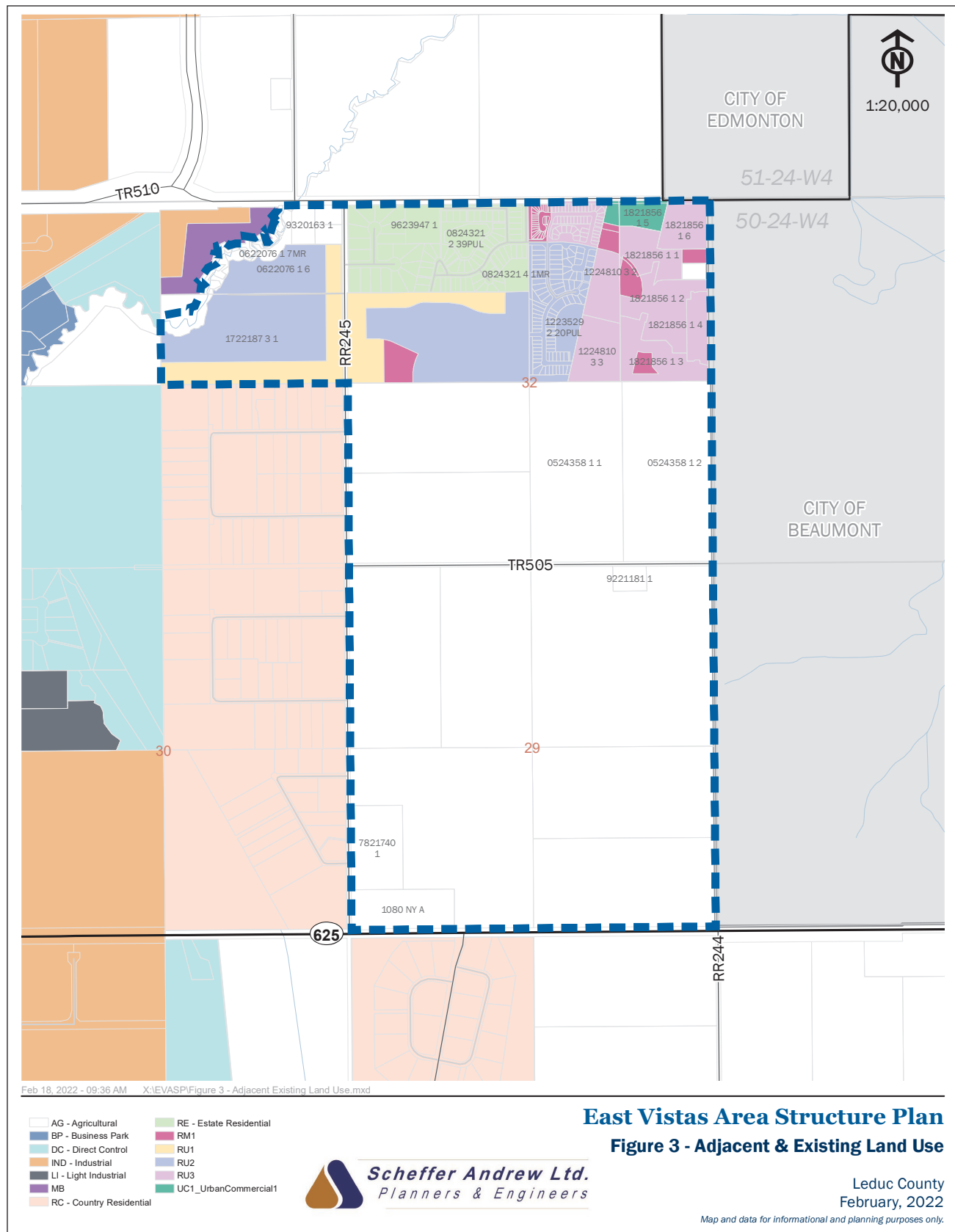
As well, the ASP is contiguous to an existing large lot municipally serviced urban residential subdivision (Lukas Estates II).

- d. Locations that utilize existing infrastructure and servicing capacity or logically and efficiently extend that infrastructure. As noted above, the East Vistas has existing infrastructure and thus meets this criterion.

In October of 2009, the Capital Region Land Use Committee released an addendum to the Capital Region Growth Plan which defined Priority Growth Areas and Cluster Country Residential Areas. Density targets were set for each of these areas. The East Vistas falls within Priority Growth Area CE, which has a density target of 25-35 units per net developable hectare of land. The East Vistas will achieve an overall density of 27.3 units per net developable hectare.

A second addendum to the Capital Region Growth Plan was released in December of 2009. This final piece of the plan revisited population and employment projections and identified new projections for Priority Growth Areas, Cluster Country Residential Areas, and municipalities over the next 35 years. An analysis of these projections as they relate to the East Vistas is presented in Section 8.1.

Figure 3 - Adjacent and existing land use



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3.2 *Natural Features*

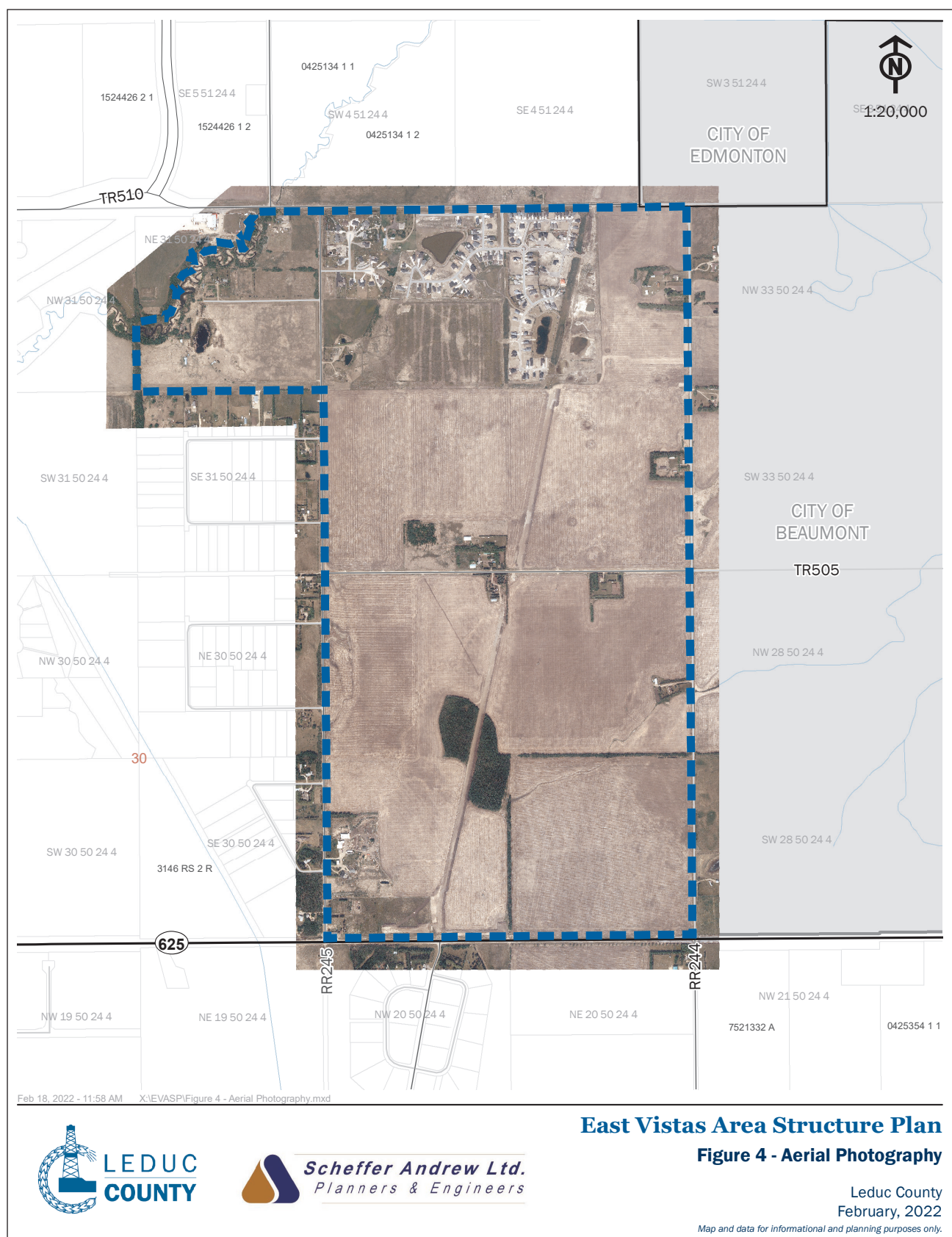
Within the plan area, the lands are predominantly cultivated farmland with flat to slightly rolling terrain. Natural areas within the plan area include wetlands, two large woodland areas, some shelterbelts around the farmsteads, and along property lines.

The drainage trend is from the northeast to the southwest with an elevation difference of approximately twenty metres across two sections. All drainage in this plan area flows into the Blackmud Creek to the west and ultimately into the North Saskatchewan River.

Irvine Creek transects the northeast quarter of Section 31 and trends from the northeast to the southwest. Alberta Sustainable Resource Development has indicated that they will be claiming the bed and shore of Irvine Creek when it is subdivided.

The natural features are shown in Figure 4 Aerial photograph.

Figure 4 - Aerial photograph



3.3 Infrastructure

3.3.1 Municipal Water

Municipal water is available from the County's regional water reservoir which is readily available by connection to a transmission line located approximately two kilometres west of the plan area. As well, a water line is available at Twp Rd 510. It was constructed by a private developer to service Lukas Estates II in the east portion of the North ½ of NW 32-50-24-4.

3.3.2 Municipal Sewer

The regional sewer line is located in the northern part of the plan area. Capacity will be available as stipulated by the policy of the Alberta Capital Region Wastewater Commission.

3.3.3 Roads

The plan area is bounded by municipal roads on the north, west, and east side which allows for ample access into the plan area. Highway 625 forms the southern boundary. Additional access to this highway will not be permitted by the Ministry of Transportation.

3.3.4 Greater Nisku Off-site Levy Bylaw

On April 2, 2008 Leduc County adopted the Greater Nisku Off-site Levy Bylaw. It provides a mechanism for the County to collect fees to front-end the development of eligible components of the infrastructure. This includes some arterial roads, water and sanitary sewer lines. The fees are collected by the County from a developer at the time a Development Agreement is completed. In turn, the County may use the fees collected to pay for approved infrastructure projects included in the County's Funded Capital Plan.

Alternatively, developers may front-end eligible infrastructure projects. A portion of the costs associated with eligible projects may subsequently be contributed to through fees collected under the Off-Site Levy Bylaw for which the County will endeavour to collect from future developers.

3.4 Plan Amendments

The County will send a copy of this plan to adjacent municipalities, public and private approval agencies, and notify the adjacent and affected land owners. The East Vistas Area Structure Plan will also be advertised for a public hearing in accordance with the *Municipal Government Act*.

To implement the East Vistas Area Structure Plan, the County must amend the Municipal Development Plan, Land Use Bylaw, and obtain the prior approval of the Capital Region Board before any of the plan amendments can be adopted.

A brief comment follows on the purpose of each of the proposed plan amendments.

3.4.1 Leduc County Municipal Development Plan

The Municipal Development Plan designates the plan area within Agricultural Area A. The Municipal Development Plan includes policies for country residential and commercial land uses and protection of environmentally sensitive lands. Rural commercial uses are intended to provide daily goods and services needed by the local population.

Amendment of the Municipal Development Plan will be needed to enable growth at a higher density with smaller, single family residential lots, as well as multi-family developments, commercial sites, and mixed residential/commercial buildings. A new land use designation will be created, to be called Urban Growth Areas. The East Vistas will be the first such Urban Growth Area in the County.

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3.4.2 Land Use Bylaw

The Leduc County Land Use Bylaw designates the plan area as AG - Agricultural District. To encourage higher density growth to proceed, new land use districts are needed in the Land Use Bylaw.

Amendment of the Land Use Bylaw will create seven new districts to regulate land use in the Urban Growth Area. Included in the regulations are design guidelines, development and landscaping standards, and servicing requirements.

A further suite of amendments to the Land Use Bylaw will address issues arising out of the establishment of full urban standard development in the County. These issues include, but are not limited to, parking, landscaping, screening, setbacks, noise, outdoor lighting, and projections.

Individual applications for redistricting, subdivision, and development are described in Section 8 of this document.

4 Development Opportunities

The East Vistas Plan area is located adjacent to municipal roads and a provincial highway which channels traffic to the Queen Elizabeth II Highway five kilometres to the west and to Highway 814 three kilometres to the east. It is in close proximity to existing employment centers within the Nisku and Leduc Industrial Parks, Edmonton International Airport, the City of Beaumont, the City of Leduc, and is a short distance from the City of Edmonton. The ground conditions of the site are conducive to development and municipal services are readily available.

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4.1 Historical/Cultural Resources

In correspondence dated May 4, 2007, Alberta Community Development indicated that a Historical Resources Impact Assessment is not required for the plan area. This correspondence is included in the Appendices.

4.2 Geotechnical Assessment

Hagstrom Geotechnical Services Ltd. prepared a general geotechnical assessment of the plan area. The report is based upon existing geotechnical reports of properties within the plan area. It concludes that there are no known geotechnical obstacles to development. The soil conditions throughout the plan area are generally suitable for the proposed range of developments. The report also recommends that some additional boreholes be drilled in Sections 29, 31, and 32 prior to the start of detailed design and construction.

The report prepared by Mr. Merle Hagstrom, P.Eng. of Hagstrom Geotechnical Services Ltd., dated September 25, 2007 has been submitted to Leduc County under separate cover.

5 Development Constraints

There are numerous development constraints within the plan area which are both man-made and natural. Figure 5 indicates natural areas and man-made constraints. Man-made constraints are the existing rights-of-way for utilities. The first natural development constraint is Irvine Creek, its valley, and required setback from the top of bank. A second natural development constraint is the existing woodlot in the south central part of the Plan area. Natural areas worthy of conservation protection are identified in an Environmental Impact Assessment report.

5.1 Man-made Constraints

Man-made constraints in the vicinity to a development are mainly related to rights-of-way. There are five rights-of-way for underground pipelines and one for an above ground electrical transmission line. The overhead line as well as two of the pipelines is orientated generally north to south. Three pipelines run east to west, with one meter station site also in the plan area. This infrastructure may present some development challenges. The north south oriented oil and gas pipelines are in two separate corridors and these lines, currently owned by ATCO Gas, Alberta Product Pipe Line Ltd., Keyera Energy Ltd. and Rangeland Pipeline Company require no additional setbacks from their respective rights-of-way. However, planting in the right-of-way and crossings are restricted and controlled.

Leduc County's Land Use Bylaw identifies that additional setback requirements dependent on the operating pressure of the pipeline may be required. The three east to west pipelines are aligned with property lines and are not considered major obstacles to development. The line adjacent to Township Road 505 is currently owned by Altagas Utilities. The line adjacent to the south boundary of the south half of Section 29 is a municipal water line and the South Edmonton Regional Trunk Sewer (SERTS) Line parallels and runs adjacent to the south boundary of the north half of section 32. As well, the Capital Region South-west Water Services Commission (CRSWSC) regional water line also runs parallel to the south boundary of the north half of Section 32.

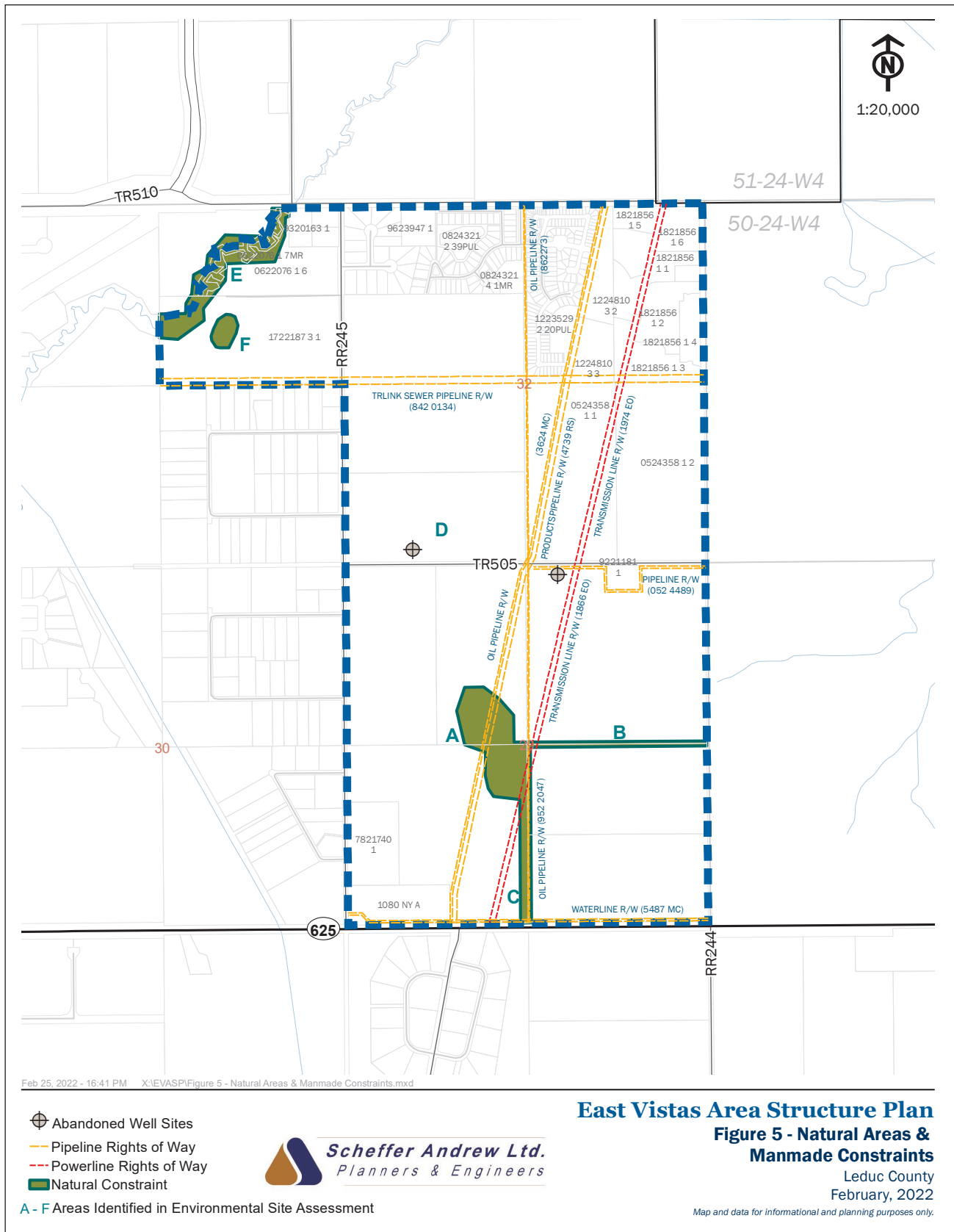
The AltaLink power transmission line is, however, a significant constraint for development both physically and for aesthetic reasons. Roads crossing this line must be designed around the transmission towers, locations, and wire heights. Altalink has indicated that it may have additional comments at the time of subdivision, depending on details of proposed new development.

With respect to sour gas well setbacks, the Alberta Energy and Utilities Board indicated in their correspondence dated March 15, 2007 that there are no sour gas facilities within or immediately adjacent to the plan boundary. There are two abandoned and reclaimed wells which will require long term access to the borehole location. Setbacks have to be consistent with the recommendations of the Alberta Resources Conservation Board. Reclamation certificates for these wells are included in the Appendices.

5.2 Phase 1 Environmental Site Assessment

A minimum of a Phase 1 Environmental Site Assessment report will be required to be completed on parcels of land to support the redistricting application for the subject parcels. The primary objective of this assessment is to identify any potential environmental contamination issues associated with the property and to determine whether a detailed site investigation is required. If a remediation plan is required, it must be completed prior to granting of the planning approvals.

Figure 5 - Natural areas and man-made constraints



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5.3 *Environmental Impact Assessment*

An Environmental Impact Assessment report was prepared by Bruce Thompson and Associates Inc. The report identifies important natural features, environmental elements in the Plan area, the site's natural sustainability, and features such as tree stands, riparian areas, or wetlands that are worthy of conservation. It also:

- identifies and assesses the potential effects of the proposed development and related activities on natural systems;
- provides recommendations for measures to avoid or reduce significant impacts to important natural elements; and,
- provides recommendations regarding the incorporation of natural features as part of the development plan.

Most of the Plan area consists of cultivated agricultural lands, residential areas, and existing farmsteads. The assessment focused on six ecosystems that remain more or less in their natural state. Six areas are identified in Figure 5 Natural areas and man-made constraints, and include:

1. a large deciduous, oval-shaped wood lot near the centre of the area, but in the south half is bisected by a pipeline labelled Area A;
2. a shelterbelt running eastward from Area A labelled Area B;
3. a shelterbelt running southward from Area A labelled Area C;
4. a small tree stand half mile north of Area A labelled Area D;
5. Irvine Creek and its ravine that traverses the northwest quarter-section of the study area labelled Area E; and
6. a small wet land area east of Irvine Creek located in the NE 31-50-24-4 labelled Area F.

The report recommends that:

- The effect of leaving Areas A, B, C, E as natural tree stands would be beneficial for the rural character, preserve some open space, add to the long-term aesthetics, maintain wildlife habitat, and preserve ecological connectivity.
- The tree stand in Area D is deteriorating severely. There is little chance it will recover. As a consequence, removing the trees will not be detrimental to the area's natural sustainability.
- Retention of Area F as a natural wetland would be beneficial. It would have long-term benefits in terms of groundwater re-charge, infiltration of water, and improvement of water quality. If the wetland is to be incorporated as a storm water management pond, then a Wetland Management Plan will be needed. Such a plan would provide any enhancement and mitigation requirements as part of the engineering design.
- The potential impacts of development on Irvine Creek could be long-term and high in magnitude. In order to minimize these effects, a storm water management plan must be designed such that the post-development drainage regime is similar to the pre- development flows. This will respond to the changes in flow rates based on the frequency, duration, and extent of flooding.
- A storm water management plan would be prepared as part of the detailed engineering design. Irvine Creek, its ravine, and adjacent riparian areas should be retained and managed as an Environmental Reserve.

The report prepared by Mr. Bruce Thompson, of Bruce Thompson and Associates Inc., dated September 2007 has been submitted to Leduc County under separate cover.

5.4 Flood Plain Analysis

River Engineering Consulting Ltd. prepared a plan to delineate the 1:100 year flood plain and determine the floodway / flood fringe limits for Irvine Creek. The report states that the floodway and flood fringe limits are totally contained within the valley surrounding Irvine Creek. The 100-Year floodplain presents no limitations to development from a flood proofing perspective.

The report prepared by Mr. Bob Quazi, P.Eng., of River Engineering Consulting Ltd., dated August 2007 has been submitted to Leduc County under separate cover.

6 Development Concept

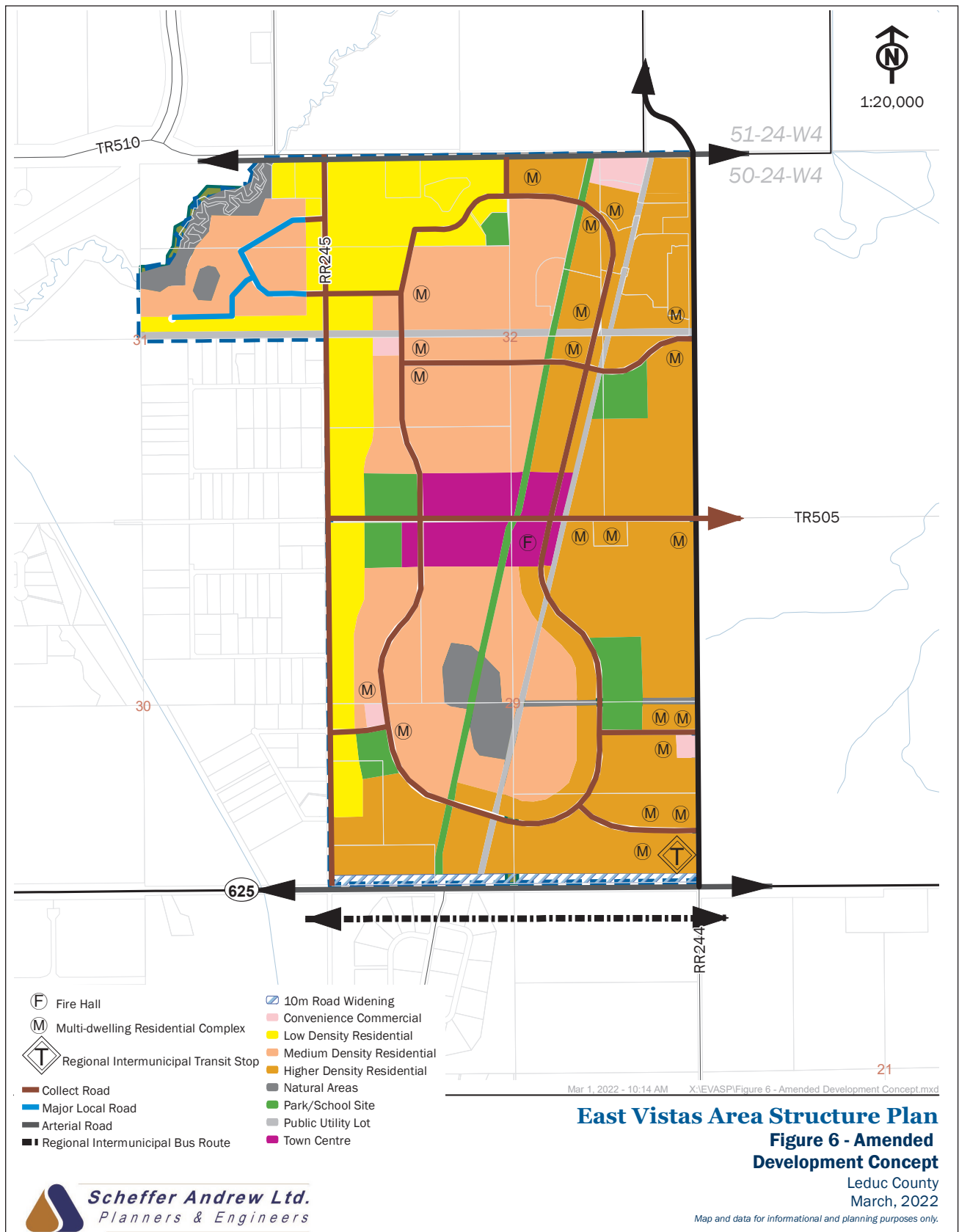
The approach used for the preparation of this plan is based on the following general planning principles:

1. Appropriate land use assignment where each land use has distinct location and activity characteristics.
2. Integration of activities with the various land uses through vehicular travel, non- motorized travel, and pedestrian linkages.
3. Clearly defined and unique neighbourhoods within easy walking distance of parks, schools, shops, amenities, and each other.
4. Respect for existing significant environmental features with a commitment to preserving and minimizing impacts to these natural amenities.
5. Provision for all of the daily needs of a complete, diverse, and vibrant community.
6. Flexibility in land uses with an emphasis on outcomes of urban form and design.

This plan will promote the development of a sustainable urban community. It includes higher density, single family residential, multi-family and mixed uses along with efficient and effective designs for the land uses, preservation of open space and environmentally sensitive areas, safety, walkability, and a strong sense of a defined neighbourhood and community.

Figure 6 shows the development concept within the plan area.

Figure 6 - Development Concept



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The Area Structure Plan statistics for capacity projections for the land use concept are as follows:

Table 1 - Amended area structure plan statistics

Land Use Distribution						
	Area (ha)					
Total Plan area	562					
Environmental Reserve/Natural Area	9					
Gross Developable Area	553.0	Net				
		Area (ha)	%GDA			
Park and School Sites		55.9	10.0%			
Stormwater Lakes		22.0	4.0%			
Public Utility Lots		18.4	3.3%			
Circulation		128.1	23.2%			
Infrastructure and Parks Area		223.8				
Vistas Centre Commercial		2.2	0.4%			
Urban Commercial		7.0	1.3%			
Commercial Developable Area		9.2				
Low Density Residential		55.4	10.0%			
Medium Density Residential		104.0	18.8%			
Higher Density Residential		112.8	20.4%			
Multi Dwelling Residential		32.2	5.8%			
Vistas Centre Residential		15.6	2.8%			
Residential Developable Area		320.0				
Total		553.0	100.0%			
Population						
Land Use	Area (ha)	%	DU/ ha	DU	PPDU	Population
Low Density Residential	55.4	18%	5.0	277	2.6	720
Medium Density Residential	104.0	33%	12.0	1,248	2.6	3,245
Higher Density Residential	112.8	35%	24.0	2,707	2.6	7,039
Multi Dwelling Residential	32.2	10%	95.0	3,059	2.6	7,953
Vistas Centre Residential	15.6	5%	100.0	1,558	2.6	4,056
Total	320.0	100%		8,849		23,013
Density		41.3	people / gross developable ha			
		72.0	people / net residential ha			
		15.9	units / gross developable ha			
		27.7	units / net residential ha			
Sustainability Measures						
Population % within 500 m of Parkland			100%			
Population % within 400 m of Future Transit Service			100%			
Population % within 600 m of Commercial Service			89%			

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6.1 Residential

New residential developments will be subject to architectural controls and design guidelines that will function to create themes unique to their development. This will reinforce a sense of identifiable neighbourhoods within the Plan area. Landscaping will be included in the regulations, as will provisions to reduce night time light pollution. All lots will be serviced by municipal water and sanitary sewer.

Figure 6 shows the distribution of residential land uses within the development concept.

6.1.1 Low Density Residential

The westernmost parts of the Plan area will be developed as single family residential. A new RU1 – Residential Urban 1 District is proposed to be added to the Leduc County Land Use Bylaw. The area will ultimately be served by a minor arterial roadway along Range Road 245, and no part of the low density housing is adjacent to Highway 625. Minimum parcel size will be 1,350 square metres (0.3 acres) with a maximum of 6,000 square metres (1.5 acres). This is shown as the single family “Low Density” designation in Figure 6.

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6.1.2 Medium Density Residential

A medium density single family residential area is located in the centre of the Plan area. This medium density development will flank the Vistas Centre on both its north and south sides, and will occupy most of the space delineated by the inner ring collector road, as well as a buffer area to the west of that road. A new RU2 - Residential Urban 2 District is proposed to be added to the Leduc County Land Use Bylaw. It has an overall density of 777 units per quarter section. The minimum parcel size will be 360 square metres (0.09 acres) with a maximum of 1500 square metres (0.37 acres). This area is shown as the single family “Medium Density” designation in Figure 6.

6.1.3 Higher Density Residential

A higher density residential area is located towards the eastern perimeter of the Plan area. As well, the higher density area forms a buffer on the west side of the inner collector road. This higher density development will be in closer proximity to arterial roads and Highway 625 than the lower and medium density residential areas. A new RU3 - Residential Urban 3 District is proposed to be added to the Leduc County Land Use Bylaw. It has an overall density of 1554 units per quarter section. The minimum parcel size will be 180 square metres (0.04 acres) with a maximum of 660 square metres (0.09 acres). This area is shown as the single family “Higher Density” designation in Figure 6.

6.1.4 Multi-Dwelling Residential

The inclusion of multi-unit dwelling sites within this plan area will allow for a more diverse range of housing types. This will accommodate seniors, young families, single parent families, childless couples, and singles; allowing residents to stay within the community when their housing needs change over time.

Multi-family dwelling sites will be located adjacent to collector and arterial roads and close to local amenities such as schools, parks, trails, commercial sites, and the Vistas Centre. The sites will connect to adjacent multi-use pathways and be situated in close proximity to single-family sites. Multi-family developments shall be directly connected to adjacent commercial and service nodes and other community amenities. Double fronted developments shall be encouraged where a multi-family lot is created adjacent to a public open space. Multi-family dwelling sites will include a range of housing forms, from duplexes to up to six connected townhouses as well as apartment structures, stacked townhouses, condominiums, and communal structures intended for homes for the aged. Affordable housing projects will be encouraged in multi-dwelling sites.

A new RM1 – Residential Multi Family District is proposed to be added to the Leduc County Land Use Bylaw. It will have a maximum site area of 1.4 ha (3.5 acres) with a maximum density of 95 units per hectare (38.4 units per acre). This is shown as the “Multi-dwelling Residential” symbol in Figure 6 -

Amended development concept.

6.1.5 Residential Design

Residential developments in the East Vistas will take on a variety of forms. However, in order to create a cohesive community, a number of design principles will apply to all developments. These are as follows:

1. The notion of ‘eyes on the street’, or orienting interior spaces such that people have the ability to casually observe the street, increases community safety.
2. To promote safety and community cohesiveness, residential development should address all fronting streets, parks, and open spaces.
3. A clear sense of entry and design interest to a home is provided through the inclusion of porches, verandas, and other architectural elements that contribute to a sense of place and activity.
4. A complete community should be able to house people of all ages, family arrangements, incomes, and cultures.
5. Successful communities create a balance between private and public space.
6. Variation in residences, structures and buildings is achieved through the use of quality materials and detail in design, which lends visual interest, distinctive character and identity to a community.
7. Minimizing the impact of the garage as viewed by the public realm creates a visual relationship between the front entrance of each home and the street.
8. Neighbourhood environments are established by the variety of architecture and landscape defined by varied lot widths and setbacks, which also allow variations in density throughout the community.
9. Consistent quality and design of landscape elements and sidewalks softens the aesthetics of structures and ties neighbourhoods together, while the addition of shade trees also increases the energy efficiency as well as the sustainability of the community.
10. Perimeter wall and entry elements provide a sense of arrival and identity for neighbourhoods. These should be creatively designed with quality materials.
11. Residential projects should be designed to maximize opportunities for creating usable, attractive, and integrated open spaces.
12. There need only be as much outdoor lighting as necessary, directed where it is needed, when it is needed and in the amount needed. All other outdoor light is wasteful.
13. Multi-family project lighting should respect the scale and character of adjacent single family neighbourhoods. It should not intrude or create a nuisance to these areas, but must maintain safety and visibility on its site.
14. Trash enclosures, loading dock, mechanical equipment and other service elements of multi-family dwelling projects should be located away from street views. Amenities should be centrally located.
15. Residential design should be consistent with respect to scale and character, but not to the point of being identical or repetitious. Variety and distinctiveness is the goal. New developments should respect the characteristics of existing developments in the area.
16. Residential development in the East Vistas should incorporate where possible site planning and building elements that promote energy efficiency and conservation of water.

6.2 Vistas Centre

A new Vistas Centre will be created as the heart of the community; shown in Figure 6 Development Concept. It will be centrally located in the plan area and will be designed according to a “Main Street” concept. The Vistas Centre and convenience commercial areas in the East Vistas are intended to satisfy the needs of the local residential community. This concept envisions a central built-up area which is dominated by a road with shops and restaurants at street level, wide sidewalks, parallel or angle parking, and attached buildings of a consistent height, bulk, and massing.

Located along Township Road 505, the Vistas Centre will incorporate portions of land both north and south of the road. The Vistas Centre is intended to be a walkable destination both through its pedestrian friendly design and its accessibility via the multi-use pathway system. As well, Main Street will be designed to accommodate a future transit system. Adjoining the Vistas Centre immediately to the west are two sites dedicated for a community recreation area and future school sites. Together with this recreation site, the Vistas Centre will provide residents and visitors with a multi-use, community-oriented destination. Multi unit residential dwellings will be incorporated into the Vistas Center at a higher density than in the rest of the plan area, and affordable housing projects will be encouraged in the district.

The convergence of two pipeline right of ways in the Vistas Centre provides an opportunity for other public uses of land. These two north-south running lines do not run parallel to one another, but rather converge at a steep angle, creating two triangular shaped areas, one on either side of Main Street in the Vistas Centre. As development is restricted on the right of ways, the pipelines will be used as elements of the multi-use pathway system in the East Vistas. At the point of convergence, however, two triangular open spaces will be created suitable for piazzas or some other public open space feature. Immediately behind these open spaces will be two trapezoidal lots that may best be dedicated as municipal reserve and developed with public service uses, such as a library, RCMP detachment, or district heating facility.

A fire hall will also be located in the Vistas Centre, as shown in Figure 6 Development Concept. The fire hall will occupy between a two and three acre site, and may include ambulatory services. It will be situated on the south side of Main Street towards the east side of the Vistas Centre, and will be adjacent to the overhead electrical transmission corridor.

The proposed land use district under the Leduc County Land Use Bylaw will be TC – Town Centre District. Permitted uses include convenience and commercial retail service, business office, financial services, and townhouse and apartment dwellings. Discretionary uses in the District include local community facilities, restaurants, child care facilities, religious assemblies, and personal or health care services. Multi-dwelling sites will be located in single use buildings off of Main Street and on upper floors of mixed use buildings along Main Street and its major cross roads. The multi-family dwelling sites will include a range of housing forms, from semi-detached to connected townhouses, duplexes, triplexes, and apartment structures either stand-alone or on upper floors of commercial buildings. The maximum density of residential development is 100 units per hectare (40.5 units per acre) and a Floor Area Ratio (FAR) of 1.5 restricts the commercial development site coverage. A single large format commercial site will be included in the Vistas Centre and is intended to accommodate a supermarket. Mixed use buildings will front public open spaces in and adjacent to the Vistas Centre.

Table 1 identifies two components of Vistas Centre development; a commercial and a residential element. It is anticipated that a majority of gross floor space in the Vistas Centre will develop as residential use rather than commercial. This reflects the fact that there will often be residential uses located on multiple stories above a commercial main floor, as well as many developments which will be entirely residential in nature. As a result, while the Vistas Centre encompasses 17.8 hectares of developable land, it is estimated that 87 per cent of this area will develop into residential uses.

6.2.1 *Vistas Centre Design*

The Vistas Centre will be the centrepiece of the East Vistas. As a result, comprehensive design standards will be applied to developments in the Vistas Centre. In order to ensure that a vibrant public realm and a consistent and attractive built environment take shape, the following principles will inform development in the Vistas Centre:

1. The Vistas Centre will be a highly accessible area in the East Vistas, whether visitors are arriving by car, transit, bicycle, skateboard, rollerblade, on foot or otherwise.
2. The Vistas Centre is the focal point of the East Vistas.
3. The public realm is the most important element of the Vistas Centre.
4. The public realm is made up of the streets, sidewalks, parks, squares or piazzas, patios, parking lots, open spaces, landscaping, and the exterior walls and roofs of all of the buildings.
5. A successful Main Street is defined by a consistent street wall close to the street, wide and friendly sidewalks, street furniture and plantings, recognizable landmarks, gateway features, public transit, and the people who live, shop, work, play, and relax there throughout the day and into the evening.
6. Living, shopping, working, playing, and relaxing are fully integrated activities in a town centre.
7. Living spaces should accommodate a wide range of lifestyles, ages, family arrangements, and incomes. Tenures will include fee simple, condominium, and rental units.
8. Buildings will generally be three to four stories on Main Street, with commercial uses at grade on major streets and commercial or residential uses above.
9. Building frontages, window openings, and architectural detailing will maintain a consistent rhythm along the front walls of buildings on Main Street and on buildings fronting onto park, civic, or open spaces.
10. Buildings fronting Main Street and park, civic, or open spaces should be attached to one another to maintain a consistent street wall.
11. The Vistas Centre is an appropriate place to showcase environmentally friendly building technologies, green roofs, energy efficient design, water conservation measures, dark sky friendly outdoor lighting, solar power, and a district heating facility.

6.3 **Commercial**

6.3.1 *Convenience Commercial*

Three local commercial sites have been located in the plan area as shown in Figure 6 Development Concept. The commercial sites are located at or near high visibility intersections and to provide convenient access opportunities. The sites are also in close proximity to multi dwelling sites and future public transit stops.

The inclusion of local commercial in the plan area is intended to minimize reliance on automobiles and encourage walking to local businesses and amenities for basic and daily needs. Local employment with a minimum commute distance will also be an advantage of these local nodes.

The proposed district designation under the Leduc County Land Use Bylaw will be CU1 – Commercial Urban 1 District. Permitted uses include convenience and commercial retail, financial services, restau-

rants, personal and health care services. The maximum allowable lot site is 2.0 hectares (5 acres) with restrictions on site coverage and individual business floor areas so as to keep the development pedestrian friendly and locally orientated.

6.3.2 Large Format Commercial

Space for a commercial plaza has been located in the northeast corner of the plan area as shown in Figure 6. This area is intended to accommodate larger format commercial uses which will serve the entire East Vistas community, as well as other County residents. Vehicular access to this development will only be provided from Township Road 510 and Range Road 244, these being the adjacent arterial roadways. Future transit routes will also stop at this commercial node. It is anticipated that this site will also provide local employment opportunities.

The proposed district designation under the Leduc County Land Use Bylaw will be CU2 – Commercial Urban 2 District. Permitted uses include convenience and commercial retail, financial services, restaurants, a gas bar, personal and health care services. The minimum allowable lot site is 2.0 hectares (5 acres). There is a restriction on site coverage but not on individual business maximum floor area.

6.3.3 Commercial Design

Commercial developments in the East Vistas are important to building a sustainable community. They need to be accessible by people arriving by a variety of modes of transportation. Once there, people outside of vehicles should feel safe and comfortable walking through parking lots and other outdoor spaces. The commercial areas should integrate well with surrounding residential activities, and will be designed in accordance with the following principles:

1. Commercial areas are for people as well as cars.
2. Commercial buildings, parking lots, and open spaces can be pedestrian, bicycle, transit, as well as automobile friendly.
3. Building orientation, façade treatment, siting, and landscaping can be used to make large buildings more human scaled.
4. People are drawn to attractive and accessible commercial areas; not by excessive, or excessively large, signage.
5. Commercial areas are spaces for informal social interaction and gatherings in addition to shopping. They should include places to facilitate these activities.
6. Ancillary, accessory, loading, and storage facilities are necessary to commercial activity, but they should not be intrusive or unattractive.
7. There need only be as much outdoor lighting as necessary, directed where it is needed, when it is needed and in the amount needed. All other outdoor light is wasteful.

6.4 Institutional

Institutional uses are encouraged in the plan area. These may include, but are not limited to, educational facilities (such as a library), recreational facilities (such as an arena), daycare operations, assisted living centres, homes for the aged, churches, and County, provincial or federal agency outlets including a Leduc County Family and Community Support Services community office. To provide a focus for community services and enhance community sustainability these uses will predominantly be situated in the Vistas Centre. The Leduc County Land Use Bylaw provides for the development of institutional uses in the new TC – Town Centre District.

6.5 Municipal Reserve / Natural Areas

Leduc County considers municipal reserves important in Urban growth Areas. In these urban environments, parks and open spaces assume a greater importance as community gathering spaces and places to play, run, wander, relax, and decompress. Full dedication of Municipal Reserve will be required through any or all of: dedication; deferral; and/or cash in lieu to best achieve the vision of the plan, at the discretion of Leduc County. - Bylaw 28-15

The Irvine Creek valley and its environs will be protected as an environmental reserve. At the time of subdivision, a detailed geotechnical report specific to the parcel being applied for subdivision will be required by the municipality. The geotechnical report will recommend the minimum setback distance for development from the top of bank. The sustainable treed areas identified by in the environmental impact assessment will be retained as municipal reserve land.

A manmade dugout identified in the NE 31-50-24-W4 may be utilized as a storm water management facility. A storm water management plan will be prepared to reduce the impact on the ecosystem of developing this site for this use.

Other opportunities for municipal reserves include the creation of a network of smaller neighbourhood park spaces. Each community as it is developed will have residential subdivision parks. Distances from a given residence to a park space will be planned such that access is maximized for all residents. Parks will be constructed by the developer and will include such amenities as are required in a Residential Subdivision Park as outlined in the Leduc County Parks Design Standards. Five large municipal reserves identified in Figure 6 Development Concept are sized to accommodate future elementary, middle or high schools. Opportunities exist to program parts of these spaces to support sports fields or arenas.

6.6 Parks and Trails

Pedestrian linkages to and from the Vistas Centre, commercial nodes, park spaces, future intermunicipal transit stop, future school sites and the balance of the developed area are important in order to facilitate a healthy community and to promote walking and cycling. Until the school sites are developed they may be utilized as local community parks. Linear pathways also provide linkages across the plan area between natural areas to facilitate people and wildlife movement. Pathways should be configured to provide connections between all natural areas, including connections to Irvine Creek.

The Leduc County Parks and Open Spaces Master Plan provides that municipal reserve credit may be given for existing utility rights-of-way if pathways are developed within them. Conversion of linear manmade constraints to pathways and green spaces results in these lands becoming assets to the community which enhance and encourage walking, cycling, and other non-motorized modes of travel. This credit will not be applied to linear right-of-ways for overhead transmission lines. Such rights-of-way will be dedicated as a Public Utility Lot (PUL). The regional sewer right of way will be dedicated as a PUL.

Stormwater management facilities and other public utility lots will be dispersed at various locations around the Plan area. Stormwater facilities are encouraged to be built to mimic natural drainage courses and wetlands, thereby increasing the aesthetic appeal of these features and the ecological integrity of the whole community. Pathways and picnic areas will also be sited to take advantage of stormwater facilities and public utility lots.

Pathways in the East Vistas will meet the Leduc County Parks Standards for a paved trail in order to be usable by pedestrians, cyclists, rollerbladers, skateboarders, people pushing strollers or wheelchairs, power chairs, and other non-motorized modes of travel. Pathways will be landscaped and vegetated and shall respect the principles of Crime Prevention Through Environmental Design (CPTED).

Opportunities for future linkages to other existing or future pathway and trail systems outside of the Plan

area present themselves to the west, north and south. Blackmud Creek lies to the west of the Plan area and may in the future feature walking paths and other park amenities. Linkages should be planned to access this natural feature using road allowances and municipal reserves.

As well, the AltaLink overhead transmission line which runs north-south through the Plan area passes close to and west of Saunders Lake, then on past Telford Lake and the City of Leduc. An opportunity exists to use this transmission right of way to create a new segment of the Waskahegan Trail, which remains incomplete through Leduc County north of Saunders Lake.

6.7 Transportation

No additional access to the plan area will be available from Highway 625. Notwithstanding this, Leduc County will continue to work with Alberta Transportation and the Capital Region Board to establish the future regional intermunicipal transit route as identified in Figure 1 Regional Transportation Infrastructure in the December 2009 Addendum of the provincially approved Capital Region Growth Plan and for which a transit stop has been conceptually identified on Figure 6 Development Concept in this plan. A functional planning study has been completed for Township Road 510 by Leduc County. This study outlines the future road expansion of Township Road 510. The Traffic Impact Assessment for the East Vistas Area Structure Plan recommends that Range Road 245 be upgraded in the future to a minor arterial standard. Traffic from the internal collector road network shown in the plan will flow to these arterial roadways at the connection points shown in Figure 6 Development Concept.

Noise and light attenuation measures will be required for residential development adjacent to arterial roads and Highway 625, which is currently under study by Alberta Transportation for future upgrade. These measures will take the form of berms, fences, and a combination thereof. Noise attenuation measures will be centred on the property line, such that half of any berm rests within the municipal road right of way and the other half forms part of final grading on adjacent private properties. Setbacks from arterial roads and Highway 625 may vary depending on the nature and extent of noise and light attenuation measures proposed, possible future road widening, and future inclusion of multi-use pathways. Such considerations will be addressed through the subdivision stage of development.

The existing Range Road 245 will function as a minor arterial and provide access to the North Vistas. Township Road 505, which connects Range Road 245 through the Vistas Centre to Range Road 244 and beyond to the City of Beaumont, will have a variable collector road cross-section; depending on the land uses surrounding it. The existing residential development in Lukas Estates II has been integrated with the collector road network.

Scheffer Andrew Ltd. was retained to provide a traffic impact assessment for the area structure plan. The traffic impact confirms the suitability of the generalized locations of the proposed collector roads and intersection with the arterial road system. Timing of intersection improvements and timing of signalization of the intersections is analyzed. An appropriate collector cross section will be utilized according to traffic generation, on-street parking, multimodal travel, and accommodating a future transit system.

Roadways in the East Vistas will be required to include traffic calming measures. Such measures will be dependent upon the roadway function and will differ between arterial, collector, and local roads. Traffic calming measures may include, but are not limited to, textured or raised cross walks or intersections, traffic circles, roundabouts, on street parking, medians, and speed humps. Traffic circles and roundabouts are preferred at collector to collector and above intersections in the East Vistas. An examination of their feasibility will be required wherever these intersections are developed.

In the future, the East Vistas is expected to support a public transit service. This service will run in an approximate figure-eight along the interior collector road and through the Vistas Centre. Stops will occur at schools, parks, collector to collector intersections, and at commercial areas. In addition, the transit

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service will link the large format commercial area and the future regional intermunicipal transit stop to the interior of the East Vistas.

In addition to local transit service, several opportunities have been identified for future intermunicipal transit service. The Capital Region Growth Plan identifies in Figure 1 of its December 2009 Addendum an intermunicipal transit route running along Highway 625 from the City of Beaumont west along the southern periphery of the East Vistas and linking through Nisku with a north-south route along Highway 2 between the City of Edmonton and the City of Leduc. This route could be extended westward to link with a potential future south extension of the City of Edmonton's LRT through Leduc County's Highway 19 Area Structure Plan lands to Edmonton International Airport. As well, the functional planning study completed by the County in 2009 for Township Road 510 included provisions for transit infrastructure, such that the northern peripheral road to the East Vistas could accommodate a transit route between Nisku and the City of Beaumont. Finally, since Main Street in the East Vistas lines up with 50 Ave. in Beaumont, a further opportunity exists for intermunicipal transit. Leduc County will explore opportunities with the City of Beaumont to jointly provide transit services between the East Vistas, the City, Nisku Industrial Park, Edmonton International Airport, and the Highway 19 ASP area.

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Any future regional intermunicipal transit stop serving the East Vistas will be configured to accommodate a 'kiss and ride' passenger drop-off facility, as well as bicycle locking facilities and sheltered passenger waiting areas. The regional intermunicipal transit stop will be serviced by both local and regional intermunicipal transit systems to ensure seamless service provision between local and regional levels of service. Direct pedestrian linkages from residential areas to all transit stops, whether local or intermunicipal, will be provided via sidewalks and bicycle lanes on adjacent roadways, and/or by pedestrian pathways.

A Traffic Impact Assessment report was prepared by Ms. Iris Ye of Scheffer Andrew Ltd dated January 2010. It has been submitted to the County under separate cover. It forms part of the technical review required by the County under the terms of reference for the preparation of this area structure plan.

6.7.1 Roadway Design

The main collector roads are identified in Figure 6. They are designed to accommodate a future transit service and improve accessibility and connectivity in the plan area, as well as dispersing the primary access routes. Bicycle paths or lanes will be incorporated into the inner ring collector roadway cross sections, on Main Street, and where possible on other collector order roads in the East Vistas. The local road network is intended to be a mix of collector roads, local roads, and rear service lanes. Cul-de-sacs will be strongly discouraged in favour of a fused grid pattern. However, where they are unavoidable, cul-de-sacs will connect to the multi-use pathway system of the East Vistas.

Each development or subdivision will address the mobility needs of residents. A plan will be provided to show local roads, pathways, and streetscape treatments. It will be designed in accordance with the following principles:

1. Integrate land uses with adjacent developments to improve connectivity and accessibility of residential areas to local parks, open space, commercial or mixed use areas, and community services.
2. Transportation infrastructure will support multiple modes of transport including public transit, bicycles and people traveling on foot.
3. Create and enhance the accessibility of and connectivity to parks, pathways, and natural areas.
4. Improve accessibility to community services and commercial or mixed use areas by providing sidewalks as well as multiuse paths which can accommodate cyclists, rollerbladers, skateboarders, people pushing strollers, powered chairs, and others.

5. Locate community services within proximity to future transit, where possible.
6. Design streets to accommodate bicyclists, motorists, transit vehicles, and pedestrians.
7. Incorporate traffic calming measures to keep streets friendly for system users who are not driving as well as improving safety for those who are driving.
8. Ensure the identified function and capacity of local transportation facilities is consistent with the Capital Region land use principles.
9. Streets and pathways should be attractive, friendly, and approachable, and not just functional.
10. Street trees, lighting, furniture, pavement treatments, landscaping, and building orientation are as important to a well designed street as the paved area and roadway cross section.
11. All street lighting and other outdoor lighting shall utilize full cut-off fixtures in order to improve efficiency and reduce light pollution.
12. Walkable streets connect to each other, have blocks that are not too long, provide a variety of points of interest, and are defined by buildings that are close, address the street, and are easily accessible.
13. Local roads, collectors, and pathways should be walkable.
14. Accessibility to all sites, neighbourhoods, and areas by emergency vehicles and emergency equipment must be ensured in every development design.
15. Roads should not negatively impact adjacent lands with respect to excessive noise, light, vibration, or airborne contaminants. They should be designed to mitigate these concerns.

The roadway network of the East Vistas will be functional, attractive, and able to support multiple forms of transportation safely and comfortably. Volumes and modes of movement will vary by time of day, day of the week, seasonally, by the relationship of the East Vistas with its neighbours, and by larger economic factors. With innovative design, attention to detail, and recognition that an automobile dependent lifestyle is neither healthy nor sustainable, the transportation elements of this Plan will contribute significantly to the creation of a vibrant and successful community in the East Vistas.

6.7.1.1 Arterial Roads

Arterial roads link the East Vistas with adjacent communities at the regional scale, strengthening the relationship between places to live, work, shop, and play. Arterial roads will be of sufficient capacity and designed to facilitate the fluid movements of persons via either private vehicles, public transit, or by a variety of alternative modes of travel. Vehicular and non-vehicular modes of travel will be physically separated. Noise mitigation measures will be required along arterial roads to shield adjacent residential areas. A typical cross section of an arterial roadway is included in Appendix C.

6.7.1.2 Minor Arterial Roads

Range Road 245 between Highway 625 and Township Road 510 will become a minor arterial roadway. As densities on the west side of this road are not expected to significantly increase over the life of the East Vistas Area Structure Plan, traffic on this roadway will be somewhat lighter than that expected on full scale arterial roads. As a result, Range Road 245 will be configured to a four lane undivided standard. As with full arterial roads, non-motorized traffic will be physically separated from vehicular traffic, with adequate spacing to provide potential future transit bus bays. A noise impact and mitigation study will be required for applicants developing along this roadway. A recommended cross section for a minor arterial roadway is included in Appendix C.

6.7.1.3 Main Street

In the East Vistas Vistas Centre, a unique collector road will act as the main street. Called Main Street, this roadway will accommodate a high volume of all modes of transport including pedestrian activity, automobiles, and a variety of motorized and non-motorized vehicles. In addition, Main Street will be designed to facilitate lingering and chance encounters. Elements such as angled or parallel on-street parking, pedestrian crossings, wide and/or sheltered sidewalks, boulevards, seating, street furniture, and human scale lighting will be present. Front patios and terraces are strongly encouraged. Roadway design will accommodate necessary elements of a future public transit system. A cross-section which is an example of an acceptable combination of elements for this collector road is shown in Appendix C.

6.7.1.4 Collector Roads

Collector roads not within the Town Center enable fluidity of movement throughout the entire Plan area. These roads will provide higher order linkages between the neighbourhoods of the East Vistas, and will facilitate access to the arterial roadways. Collector roads will feature on-street parking, pedestrian crossings, bicycle lanes and/or multiuse pathways, sidewalks, boulevards, street trees, transit infrastructure, and human scale lighting. While the functionality and safety of the roads are paramount, collector roads must also be attractive so that they remain walkable. Lots will thus front onto collector roads. In Appendix C, a cross-section of an example collector road with an acceptable combination of elements is depicted.

6.7.1.5 Local Roads

Local roads serve a number of functions in addition to providing access to individual lots. They are children's play areas, informal meeting spaces, work areas, and canvases for artistic expression. Local streets will be designed not only to accommodate the interaction of neighbours, but to celebrate neighbourhood spirit. Elements such as on-street parking, pedestrian crossings, bicycle lanes and/or pathways, sidewalks, boulevards, traffic calming mechanisms, and lighting will be carefully planned to ensure both a very high level of safety for all users and a high degree of connectivity within the neighbourhood.

As local roads provide the fine grain of the transportation system, their network configuration must be addressed. To be walkable, blocks should be less than 150 m, streets should connect to one another, users should be able to maintain their sense of direction, and travellers should have more than one route option to get to their final destination.

The precise layout of local roadways will vary from neighbourhood to neighbourhood and from residential areas to mixed use areas, with progressively more elements included along streets closer to Main Street. In Appendix C, an example of a local residential road with an acceptable combination of elements is depicted.

6.7.1.6 Lanes

In the East Vistas, rear lanes will be provided in the Vistas Centre, as well as in residential neighbourhoods. Whenever possible, lanes will also be provided along collector roads and in multi-family districts. This transportation network element will be designed to facilitate access to the rear of lots, parking facilities, and loading areas, and to enable buildings to front onto collector roadways. They will also provide access to municipal service vehicles for policing, fire protection, and solid waste pick-up. Lanes may also accommodate utility rights-of-way. A typical cross section of a laneway in a residential area is included in Appendix C.

6.7.1.7 Multi-use Pathways

Multi-use pathways and sidewalks will ensure that residents and visitors are able to use alternative modes of transportation such as rollerblading, cycling, skateboarding or walking to travel through their community. Connectivity, practicality, comfort, and safety will be the prime considerations in the design of pathways. There must be both origins and destinations for a pathway system to be successful. Maintaining consistency of surface treatments between segments of pathway and sidewalk is important to illustrate the intended use of each. Elements such as surface material, accessory furniture, lighting, and access management will be closely scrutinized in order to ensure that the East Vistas pathway system is a valued asset of the community. Pathway lighting will utilize full cut-off fixtures and may also include motion sensor activation. Pathways will be designed and constructed in accordance with Leduc County Parks Design Standards. A typical pathway is included in Appendix C.

6.8 **Municipal Services**

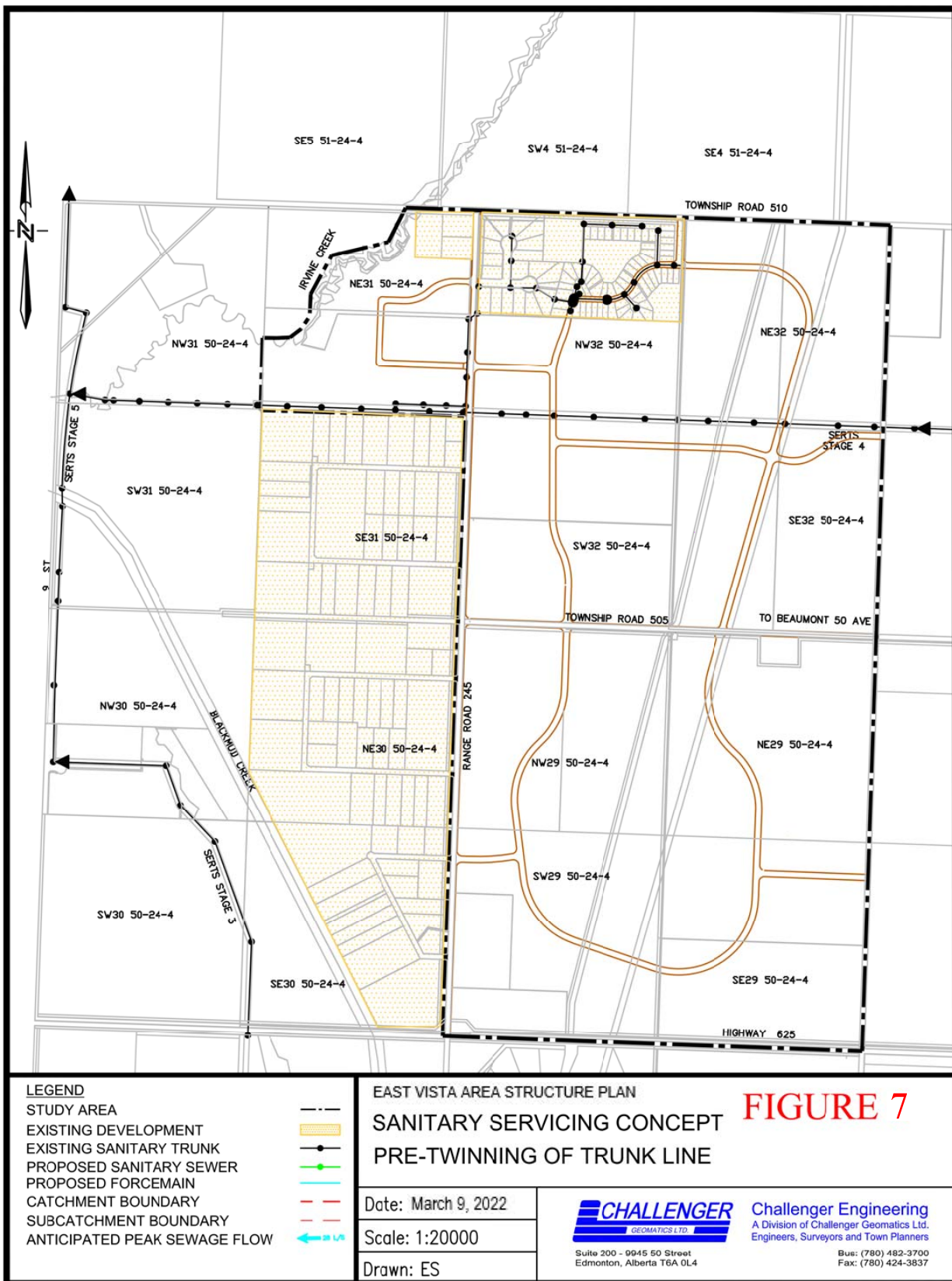
6.8.1 Sanitary

Two existing Southeast Regional Trunk Sewers (SERTS) will service the site. The northern portion of the site can presently be serviced by gravity by the existing 525mm SERTS Stage 4 Trunk main which transects the site in an east-west direction in the center of Section 32-50-24- W4. The extent of the service area of this northern catchment will be enlarged with the completion of the planned twinning and deepening of the SERTS Stage 4 Trunk main – a 900mm sewer scheduled for staged construction beginning in 2010 and anticipated to be completed in 2012. The twinned line would thus extend the south boundary of the northern catchment to approximately Township Road 505.

The southern portion of the site cannot be serviced by gravity from the SERTS Stage 4 Trunk main, but must be serviced by a newly constructed lift station which will be located in close proximity to the southwest corner of the SW quarter of Section 29-50-24-W4. This lift station will discharge into the north-south SERTS Stage 3 Trunk main via a force main to be located north of the Highway 625 road right of way.

Figures 7, 8, 9 and 10 indicate the sanitary servicing concepts, catchment areas, and possible service locations. The mandate of the Regional Sewer commission requires it to provide service to the area structure plan provided that it meets the level of service flow criteria. The East Vistas ASP recognizes that additional right-of-ways may be required to accommodate the SERTS line twinning. Right-of-way requirements will be identified at the Outline Plan stage and acquired at the subdivision stage of development.

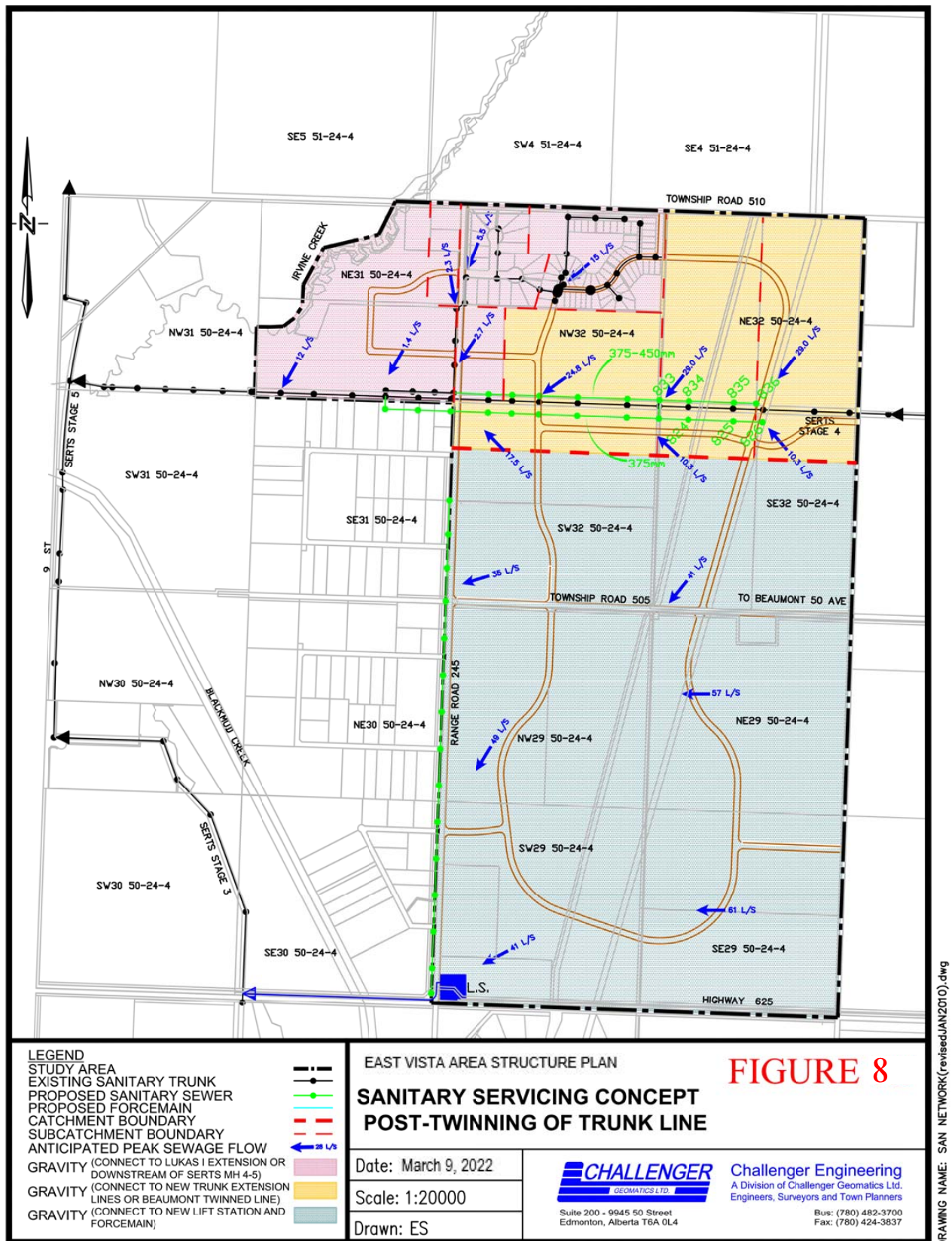
Figure 7 - Sanitary servicing concept: pre-twinning of trunk line



DATE PLOTTED: March 9, 2022

Bylaw 12-22

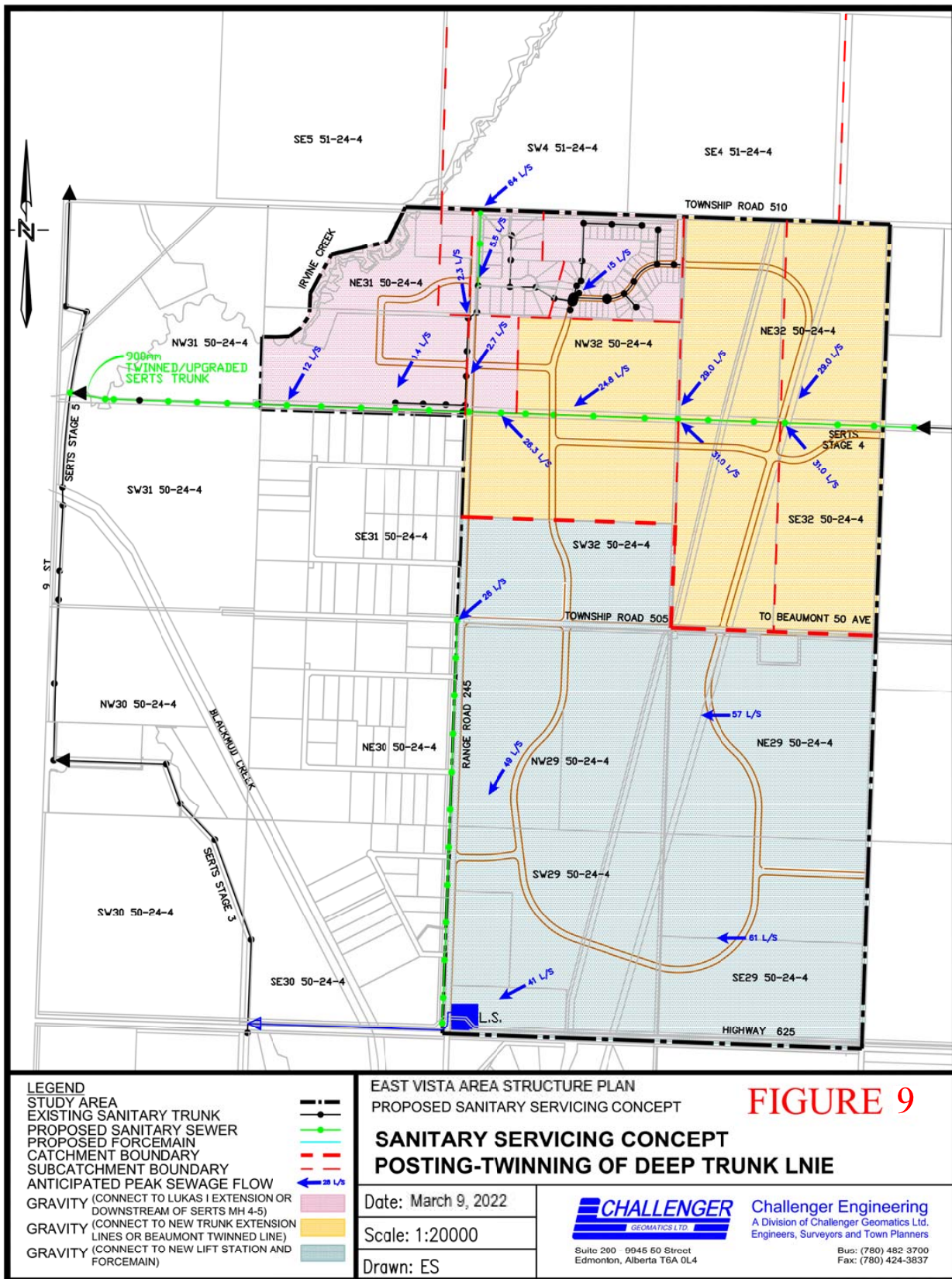
Figure 8 - Sanitary servicing concept: post-twinning of trunk line



DATE PLOTTED: March 9, 2022

Bylaw 12-22

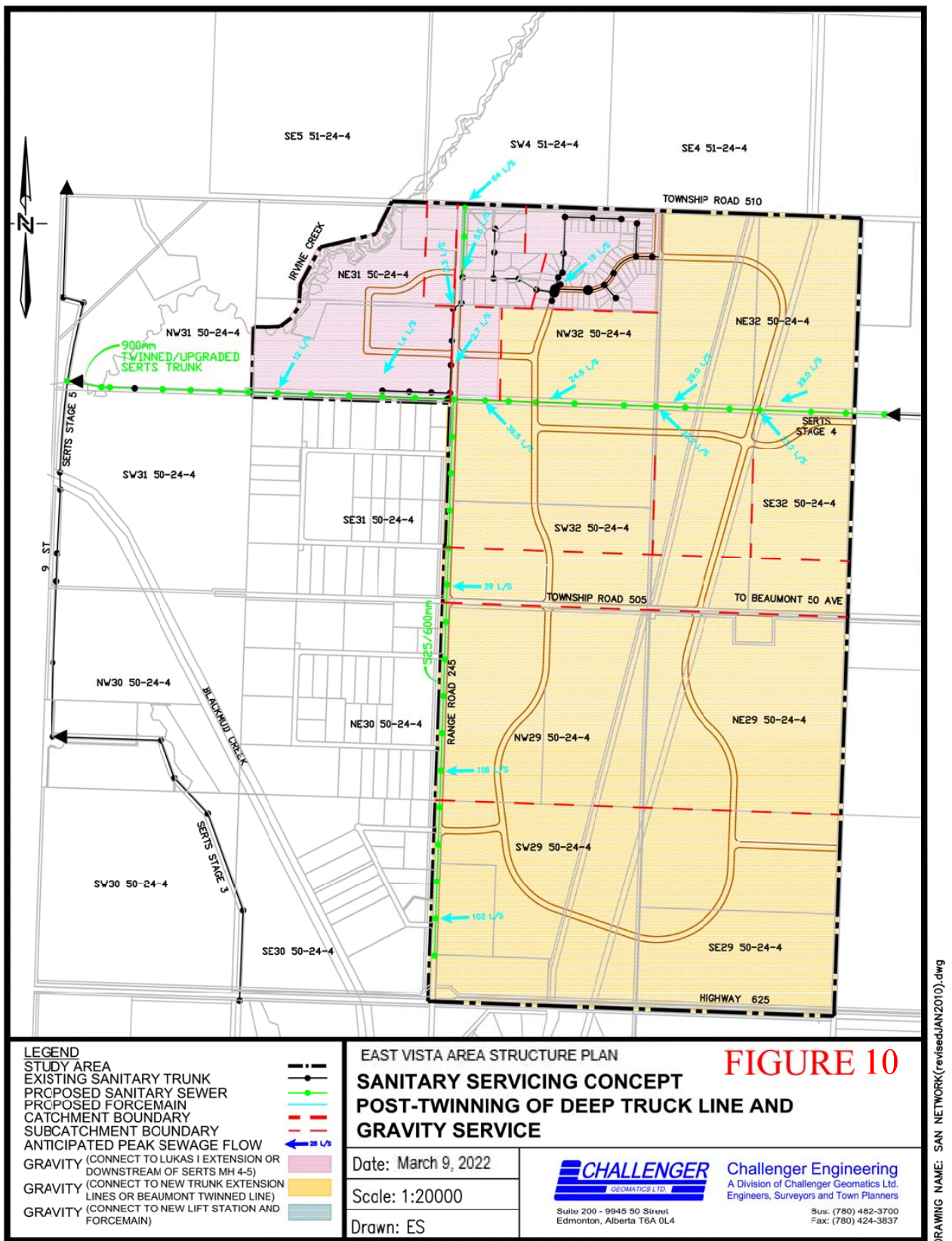
Figure 9 - Sanitary servicing concept: posting-twinning of deep trunk line



DATE PLOTTED: March 9, 2022

Bylaw 12-22

Figure 10 - Sanitary servicing concept: post-twinning of deep truck line and gravity service



6.8.2 Water

A 300 mm water main exists in Township Road 510 at the Lukas Estates II development in the East Vistas Plan area. Highway 625 currently has an existing 200mm water main that is to be upgraded to a 400 mm. Also, the CRSWSC regional water line will be twinned with a 600 mm pipe installed from the Nisku Eastern Pumphouse to the plan area. Water will be distributed throughout the site through water mains located in the plan area's collector road network. This water distribution network will consist of a 300 mm and 400 mm water main loop within the collector road and 200 mm, 250 mm, or 300 mm water mains to connect to the existing water mains along the plan area boundaries. Figure 11 depicts the water servicing concept. Additional right-of-ways may be required to accommodate the CRSWSC regional water line twinning. Right-of-way requirements will be identified through the Outline Plan stage and acquired at the subdivision stage of development.

6.8.3 Stormwater

Local stormwater management facilities will be located throughout the area structure plan in accordance to the topography and post development drainage. Each stormwater management pond should serve a minimum of eighty acres, and wherever possible, emulate existing drainage patterns.

These storm water facilities will provide storage of excess runoff from developments, and shall control discharge into the existing drainage channels to pre-development levels. These facilities will have the capability of providing sedimentation and hydrocarbon pollutant removal.

Figure 12 indicates where the storm water facilities may be sited. Stormwater facilities will be registered as Public Utility Lots, including all identified flood overflow areas. Municipal Reserves are encouraged around stormwater facilities in order to facilitate public access and enjoyment of these amenities. Trails, pathways and other park amenities in these reserve areas will be determined through consultation with Leduc County Parks and Recreation.

All developments shall be able to demonstrate that proposed stormwater management facilities have been designed using the principles of Low Impact Development (LID). The major categories of LID strategies are outlined as follows:

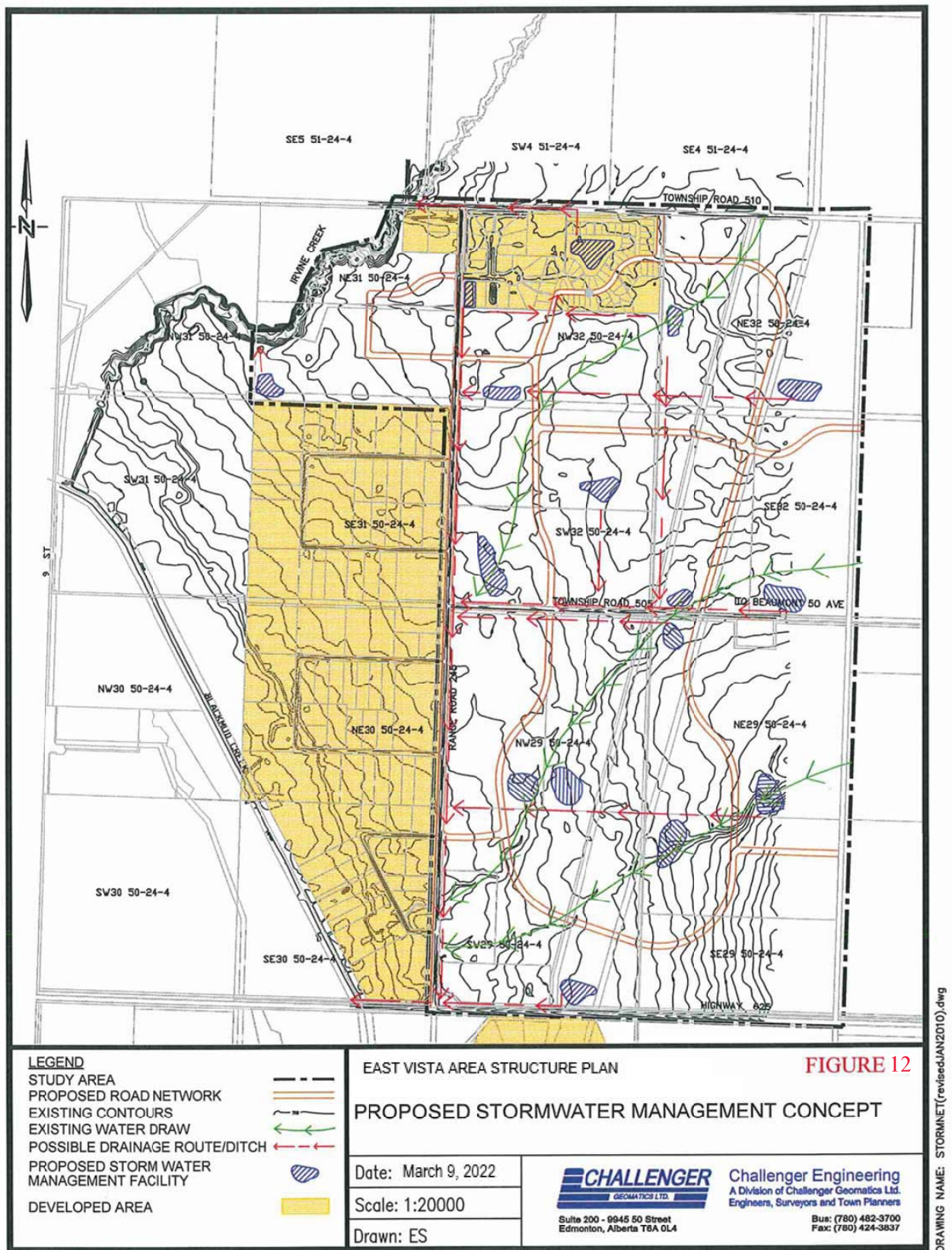
1. Bio-Retention Systems - include a high filtration rate, engineered soil and plants that process and filter pollutants. The systems can be designed as conveyance, detention, or retention systems.
2. Filtering Technologies – designs used to trap or treat pollution by controlling the flow of water.
3. Permeable Pavements – pavement systems that include a permeable surface and subsurface to infiltrate runoff and are capable, if required, of supporting cars and trucks.
4. Site Design Strategies – strategies that minimize the change in how water moves over the land through clustering of development, flow path disconnection, surface change, and lengthening.
5. Soil Amendments – addition of organic and other materials to soil and mechanical methods that help restore the infiltration capacity of the soil.
6. Vegetative Systems – planting of vegetation or creating vegetated buffers to filter or absorb rainfall and runoff.
7. Water Conservation/Reuse – the capture and storage of runoff for secondary uses such as irrigation, cooling, and other potable and non-potable uses.

Challenger Engineering prepared a Municipal Servicing Study dated January 2010 which has been submitted to Leduc County under separate cover to support this Area Structure Plan for water and sanitary servicing and stormwater management.

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Figure 12 - Proposed stormwater management concept



6.9 Franchise Utilities

Franchise utilities such as natural gas, telephone, and electricity are readily available and will be extended as development occurs. Telus has indicated that they will be able to offer high speed internet service in the East Vistas, initially with copper cabling and ultimately through the latest fibre-based technology. Upgrades to the telephone, cable, and internet system will be dependent on the East Vistas' growth and corresponding increased consumer demand.

6.10 Community Services

6.10.1 Emergency Medical Services

Alberta Health Services has indicated that the additional calls generated will be within its current capacity. Leduc County Fire Services has added that a future East Vistas fire hall could also accommodate an ambulance dispatch facility if required.

6.10.2 Leduc County Fire Services

Fire services are currently delivered to the East Vistas plan area by Leduc County Fire Services from the Nisku Fire Hall, located at 606 21st Avenue, Nisku; approximately 2.4 km from the nearest point in the plan area. Currently, the fire service is a volunteer force with a close partnership to the Edmonton International Airport fire force, which operates a small but expanding full time service. Leduc County Fire Services has indicated that a move to a different service delivery format would be predicated on the preparation of a County wide Risk Assessment. This exercise is expected to be conducted sometime in the next 2 – 5 years.

Presently, no part of Leduc County rests within a ten minute fire response time. The Alberta Building Code specifies standards of construction depending on building separations in areas outside of a ten minute fire response time. When a fire hall is constructed within the East Vistas and is manned with a full time staff, response times will be reduced to less than ten minutes.

In the future, Leduc County Fire Services has indicated that it wishes to construct a new fire hall in the East Vistas. The details of the development on the site of this facility will be arrived at through further study, but the site's location and approximate size have been identified in consultation with the Fire Service. The conceptual location of the future East Vistas Fire Hall is shown on Figure 6 Development Concept.

6.10.3 Hospital Services

The Grey Nuns Community Hospital does not have any concerns with the Area Structure Plan. The Leduc Community Hospital likewise has not indicated any concerns.

6.10.4 Policing Services

The Leduc detachment of the RCMP has indicated that it will respond to future development in the plan area from its current base in the City of Leduc. Leduc County Protective Services indicated that it will eventually need to open an outlet for its services in the East Vistas. The timing of the new facility in the East Vistas will depend on the pace of growth in the plan area, the availability of leased space, and/or potential shared facilities with the RCMP.

The focus of activity at the East Vistas Protective Services outlet will be bylaw enforcement. As development activity increases in the plan area, additional bylaw enforcement officers will be hired. Currently, the department is operating at capacity. At the time of the writing of this plan, the department was undertaking a level of service review. The outcomes of the review will determine the needs of the department in the near, medium, and long term future with respect to staffing, equipment, and facilities. The review will also inform the timing and location of the new service outlet.

Currently, Leduc County Protective Services and RCMP jointly provide policing services in the County. The main function of the County's peace officers is to patrol roads, respond to ratepayer complaints, and carry out the County's bylaw enforcement role. In the future, it is expected that the RCMP will provide a full range of policing services in the East Vistas. The County's peace officers will continue to provide their current range of services in rural areas, but will focus their efforts in the East Vistas on bylaw enforcement.

6.10.5 Recreation Facilities

Recreational facilities in the East Vistas will be developed in coordination with the City of Beaumont and possibly the City of Leduc Parks and Recreation Departments in order to avoid duplication or gaps in the regional inventory of facilities. This coordination may result in a partnership between two or three municipalities and the creation of regional parks and recreation networks.

Currently, recreation needs of County residents in a wide area that includes the East Vistas are accommodated by the City of Beaumont, with subsidy funding provided by Leduc County through an agreement with the City on a per capita basis. While many of the East Vistas' recreation space needs will be met by public use amenities on school sites in the plan area, it is recognized that a large recreation centre will ultimately be needed as the population of the community grows beyond 10,000 residents.

Both Leduc County and the City of Beaumont have identified an opportunity for the two municipalities to acquire lands for the joint development of a recreation facility and/or for future school dedication somewhere in the area between the City and the East Vistas (Sections 28 or 33 50-24-W4). It is estimated that a minimum of 14-16 hectares (35-40 acres) will be required for a recreation facility and its associated parking area that is capable of serving both the East Vistas and the City of Beaumont. Issues to resolve in this joint venture will include land acquisition, final location of the recreation facility, cost and revenue sharing for land purchase, construction and subsequent operation of facilities, and coordination with Black Gold Regional Schools and St. Thomas Aquinas Catholic Schools for any school site dedications.

As the East Vistas grows, Leduc County will periodically review its funding agreement with the City of Beaumont. At some point in the future, and depending on the rate of growth and schools development in the East Vistas, the County will begin to provide its own recreation services to area residents. This will trigger a full review of the funding agreement between the County and City, and may result in a cooperative approach to facilities provision and funding.

6.10.6 Libraries

Currently, library services in the East Vistas plan area are supplied through the Beaumont Public Library, which operates a 4400 sq. ft. facility adjacent to the Beaumont Town Centre, constructed in 2004. Libraries in municipalities within Leduc County are a part of the Yellowhead Regional Library system. Leduc County provides a subsidy to its urban municipal neighbours in return for access to library services for its citizens. The East Vistas lies within the catchment area of the Beaumont library service.

While library services are increasingly accessible via the internet, a need for physical library branches nevertheless remains for the foreseeable future. In the East Vistas, a library branch may be provided as part of a school site, in a recreation centre, through commercial space leasing, or as a stand-alone service. Opportunities have also been identified through consultation with Black Gold Regional Schools as well as St. Thomas Aquinas Catholic (STAR) Schools for partnerships in library service provision in the East Vistas. As East Vistas grows, Leduc County will continue to work with the City of Beaumont and the two School Boards to ensure that efficient, effective, and sustainable library services remain available to all area residents.

Bylaw 12-22

6.10.7 Family and Community Support Services

Leduc County Family and Community Support Services (FCSS) have indicated that East Vistas will represent a major change to its programming. As development proceeds, urban residents will demand more comprehensive services. Many programs which are currently offered in-home will more efficiently be delivered in the East Vistas from a central location. A new Community Office in the Vistas Centre will be required, as well as a significant expansion of staff resources, including a youth worker. Leduc County FCSS will examine opportunities to partner with the City of Beaumont FCSS in order to harmonize program development and delivery across these two urban areas, and to realize efficiencies of scale.

6.10.8 Schools

The population in the East Vistas ASP will be 23,013 people at full build-out. The overall student population in the plan area would be 4,543 based on the below estimate of student generation estimates. It breaks down as follows:

Table 2 - Amended student populations

Student population	
Kindergarten - elementary students (K - Grade 6)	1,611 (7% @ 23,013 population)
Junior high students (Grade 7 - 9)	1,611 (7% @ 23,013 population)
Senior high students (Grade 10 - 12)	1,381 (6% @ 23,013 population)
Total number of students	4,543

Bylaw 12-17, Bylaw 23-17

The municipal reserves in the East Vistas will be approximately 54.4 ha based on the gross developable land area. Black Gold Regional Schools and St. Thomas Aquinas Roman Catholic Schools (STAR) have expressed their needs for future school sites based on the above student generation rates and agreed to sizing of reserves as identified in a 2007 school reserves agreement between the respective School Boards and Leduc County. As per the agreement, minimum school sites shall be:

- 3.2 ha (8 acres) for an elementary school
- 3.2 ha (8 acres) for an elementary to junior high school
- 3.2 ha (8 acres) for a junior high school
- 4.9 ha (12 acres) for a junior to senior high school
- 6.1 ha (15 acres) for a senior high school with less than 500 students
- 8.1 ha (20 acres) for a senior high school with more than 500 students

Twenty five hectares of sites suitable for school reserves have been identified in Figure 6 Development Concept. Each of these sites may also serve in part or in whole as a municipal park reserve. However, priority will be given to use for schools. Additional school sites will be identified when the need arises and as development proceeds through the subdivision process.

STAR Schools has indicated through consultation that it sees an opportunity to develop a regional school to serve both the East Vistas and the City of Beaumont. This school may best be sited somewhere in between the City and the plan area, in Sections 28 or 33 50-24- W4. Should this opportunity be acted upon, the School Board may explore partnerships with the City and/or the County to jointly develop a recreation centre along with the school site.

Bylaw 12-22

7 Public Input

On July 31, 2007 Leduc County notified all the land owners in and adjacent to the plan area of its intention to proceed with the development of the “East Vistas” Area Structure Plan. Two phone calls were received by the Administration in response to the mail-out inquiring whether or not additional opportunities for public input would be available.

Advertising was placed in the local community newspaper and Leduc County notified all the land owners in and adjacent to the plan area of a public meeting that was held at the Nisku Recreation Center on January 16, 2008. The purpose of the meeting was to obtain the input from landowners and any other potentially affected parties regarding the concept plan and to identify any concerns with the proposed development concepts within the plan area. The meeting was conducted in an Open House format with display boards with representatives of the consultants and County available to provide one on one feedback for questions. In response to numerous inquiries, a verbal summary of the concept plan and “Smart Growth” planning principles was given to the attendees.

A brief Question & Answer session followed. Sixty two people signed the attendance sheet. An exit survey was filled out by twenty seven of the attendees. There was overwhelming support for the proposed concept plan, distribution and proportion of uses in the plan area. A copy of the survey results is attached for reference in the appendices.

Comments from the individual landowners for specific changes to the plan were reviewed by the Administration of Leduc County on February 6, 2008. Revisions were made where technically feasible and if consistent with the plan principles.

A second open house was held on September 22, 2009 at the Nisku Inn to provide the public another opportunity to comment on the proposed plan prior to the public hearing scheduled for October 6, 2009. Eighty two people signed the attendance sheet and fifteen exit surveys were completed. Again, there was strong support for the proposed concept plan, distribution and proportion of uses in the plan area based upon verbal comments and exit surveys. A copy of the survey results is attached for reference in the appendices.

On February 24, 2010 a third open house was held at the Nisku Inn. Registered attendance was 43 people. At this open house, there was a strong representation of elected officials and administration from adjacent communities. This open house was held to revised concept plan and district regulations which exceed the density target set by the Capital Regional Board target for this Priority Growth Area; released in mid October 2009. A copy of the survey results is attached for reference in the Appendices.

A public hearing was held at Leduc County Council Chambers on July 22, 2010 at the regular Council meeting. No concerns were brought forward by the public.

8 Implementation

8.1 Growth Assumptions & Projections

In June of 2009, the Minister of Municipal Affairs accepted the Capital Region Growth Plan which included projections of population for the Capital Region, including the East Vistas plan area, for the next 35 years. According to that plan, Leduc County will experience an annual average growth rate of between 1.6% and 4.4%. These figures reflect the trend and alternative scenarios of population projections, as determined by the Capital Region Board.

The trend scenario projects population into the future based on past growth trends. It assumes that future growth will occur in the Capital Region in the same way that past growth has occurred. The alterna-

tive scenario bases population figures on the assumption that future growth will not occur as it has in the past, and will rather follow smart growth principles, which focus on compact growth in serviced areas.

Because the East Vistas represents a land use not yet in evidence in Leduc County, it is difficult to accurately determine population growth in the plan area over the life of the plan. However, some potential general projections may yet be made, as follows.

According to the Capital Region Growth Plan, Leduc County's expected population in 2044 will be between 20,110 people (trend scenario) and 22,957 people (alternative scenario). This represents a growth of 5798 people (trend) or 8645 people (alternative) over the 2009 County population of 14,312. In terms of annual growth, it translates to 165 (trend) or 247 people (alternative) per year, for an annual growth rate of 1.4%, as stated in Table 2 of the December 2009 addendum to the Capital Region Growth Plan.

If we assume that the East Vistas absorbs 70% of Leduc County's growth over the next 30 years, we can project an annual growth of between 116 and 173 people in the plan area. Over ten years, this results in a population of 1160 – 1730 people. In twenty years, that population expands to 2320 – 3460 people. And in thirty years, there may be 3480 – 5190 people living in the East Vistas.

The December addendum to the Capital Region Growth Plan also makes growth projections for each Priority Growth Area (PGA) in Tables 3 & 4. The East Vistas, southeast Edmonton, and the City of Beaumont all fall within PGA CE. Growth for this PGA is expected to be 23,333 people over 35 years, for an average annual growth rate of 2.9%. Table 3 indicates a 2009 population of 13,430 in PGA CE, of which 11,794 reside in the City of Beaumont. This leaves a population of 1636 living in other parts of PGA CE. With a projected growth rate of 2.9%, the population in PGA CE outside of Beaumont is projected to be 2177 in ten years, 2898 in twenty years, and 3857 in thirty years.

Tables 1 & 3 of the December addendum project the population of each municipality in the Capital Region (Table 1) and each PGA (Table 3) over the 35 year life of the Growth Plan. If one subtracts the projected population of the City of Beaumont from that of PGA CE, a third set of projected populations is arrived at for the PGA, of which the East Vistas represents a small portion of land area. These are: 2729 people in ten years, 5176 in twenty years, and 15,186 in thirty-five years.

Of course, not all growth in PGA CE that lies outside of the City of Beaumont will occur in the East Vistas over the next thirty years. A significant part of the PGA rests within the City of Edmonton, where development is already well underway in the south portions of the Ellerslie Area Structure Plan. Given the established market for urban form development in the City which is currently absent in Leduc County, as well as the more advanced system of planning in Edmonton, it is reasonable to assume that in the first ten years 40% of the growth in PGA CE outside of the City of Beaumont will occur in the East Vistas plan area, and the remaining 60% will go to the City of Edmonton. This renders a population projection of some 1092 people in the East Vistas in ten years. If it is assumed that the above noted deficiencies in the County are redressed in the next ten years, then one can assume that growth in PGA CE will be shared equally between the City of Edmonton and Leduc County beyond the ten year horizon. This renders projections of 2588 people in the East Vistas in twenty years, and 7593 in thirty-five years.

From these analyses emerges a range of potential future populations in the East Vistas, as follows:

- 1,160 – 2,177 people in ten years
- 2,320 – 3,460 people in twenty years
- 3,480 – 7,593 people in thirty to thirty-five years

As is clear from these projections, the ultimate build-out population of the East Vistas will likely only be reached at some point well beyond 30 years from the adoption of the Area Structure Plan.

Bylaw 12-22

8.2 Development Sequence

Development in the East Vistas is expected to begin in the north central part of the plan area and proceed south and east. This development will initially be entirely residential, as commercial development tends to follow the market rather than lead it. As well, it is anticipated that multi-family residential development will lag somewhat behind single family and duplex construction. This is evidenced in current development patterns found in the Edmonton area.

At some point in the future, a section of sewer main running from the plan area west along Highway 625 to the Nisku trunk will be constructed. This will be accompanied by the construction of a lift station at the southwest corner of the plan area. At that time, development of lands within the south drainage basin will become feasible. This second front of development will proceed from the southwest corner of the plan area toward the north and east.

It is undetermined at this time how the mixed use Vistas Centre will develop. Because mixed use building form is currently unfamiliar to the development community in Leduc County, this may function to delay construction. Other factors that may inhibit mixed use construction are building code requirements, developer perceptions of demand for mixed use construction, and the cost of construction of public amenities in the Vistas Centre. However, mixed use development is also one of the major trends in land use planning and urban development. It is strongly encouraged in much recent planning theory and practice. This push to build new mixed used developments may balance some of the inhibiting factors outlined above.

Ultimately, successive staging of subdivisions and development will be based upon market demand and extension of the land uses, roads, utilities, and services.

8.3 Infrastructure and Roads

Leduc County policy is that development will proceed on the basis of the availability of adequate road access and, in Urban growth Areas, municipal water and sanitary sewer. This reflects the County's policy requiring proponents to take full financial responsibility for the extension of all municipal utilities and services required to support their development.

An approved Traffic Impact Assessment (TIA) specific to a subdivision application will be required by Alberta Transportation and Leduc County prior to going forward to the Subdivision Authority. An update to the East Vistas TIA may also be required as determined by Alberta Transportation. Since each stage would have its own schedule to reach its full development, the TIA update will need to pin point as best as possible the anticipated year when warrants for signals would be met at each of Range Road 244 and 245 intersections with Highway 625. Inclusion of the warrants for the current stage of development will be required, as will clarification of this stage of development relative to the overall East Vistas concept, when the TIA and TIA update is submitted.

For parcels adjacent to arterial or minor arterial roadways, a noise mitigation study will be required by Leduc County at the time of subdivision. This study will determine the degree of mitigation required, as well as the form mitigation measures will take. Construction of these mitigation measures shall be a condition of subdivision approval.

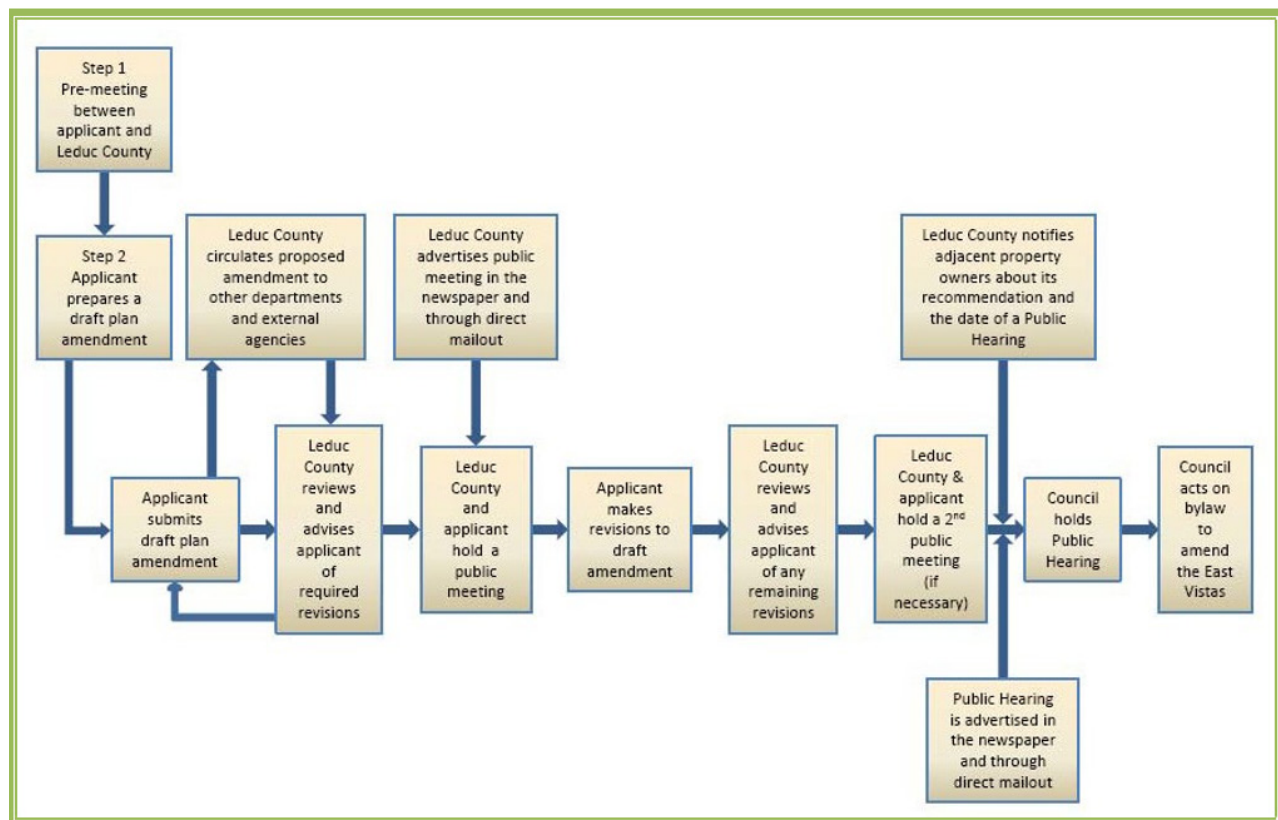
8.4 Plan Review

From time to time, the East Vistas Area Structure Plan will require a review and update. Alternatively, a proponent may request to make an amendment to the plan. In the former case, a number of triggers may be identified which may initiate a plan review. These include:

1. Significant changes to the Capital Region Growth Plan.
2. Significantly new, unexpected, or large developments in the Capital Region, either through private investment or public expenditure on infrastructure, such as major transportation infrastructure.
3. Dramatic changes to energy consumption patterns, such as those which may result from fossil fuel shortages and high pricing, or the mass marketing of alternative energy sources.
4. Technological advances that may affect land use patterns, employment, housing, or transportation.
5. Major changes in transportation behaviour as a result of any of the above.
6. Major changes in Provincial legislation governing municipalities or land use planning.
7. Major restructuring of Leduc County or municipal reorganization imposed upon or affecting Leduc County from the Provincial or regional level.
8. Potential future joint planning with the City of Beaumont. - Bylaw 12-22
9. At the discretion of Leduc County Council.

8.5 Plan Amendment

Amendments to the East Vistas ASP will be subject to the process outlined in the flow diagram below.



This begins with a pre-meeting between the applicant and Leduc County Planning & Development. At this meeting, the proposed amendment will be discussed. Following the meeting, Leduc County will notify the applicant in writing of the requirements of the amendment submission. Once the submission is tendered to the County, a review and revision process will begin which may involve several iterations of the submission before a public meeting is scheduled.

Following the public meeting, a further period of revision will take place, followed by a review by Leduc County administration and a possible second public meeting. Whether a second meeting is required will depend on the nature and extent of changes to the proposed amendment following the first public meeting.

Once all concerns have been addressed to the satisfaction of Leduc County administration and all supporting studies and material have been finalized, a public hearing date will be set and advertised. At the hearing, County administration will make a recommendation to County Council, who will also listen to final public comments, including any made by the applicant. Council will then vote on a bylaw to accept the amendment.

8.6 *Redistricting Applications*

When a redistricting application is tendered to the County, the applicant will, at a minimum, be required to provide the County with an Outline Plan. Requirements of such plans shall be as per Leduc County standards and are available from Leduc County Planning and Development.

8.7 *Subdivision and Development Permit Applications*

Applications for subdivisions and/or development permits must meet all Leduc County requirements as set out in the Leduc County Land Use Bylaw and other documents.

East Vistas Area Structure Plan

Appendix A

**Historical resources impact assessment
response**



a.stewart

From: Margret Ingibergsson [Margret.Ingibergsson@gov.ab.ca]
Sent: Friday, May 04, 2007 11:23 AM
To: a.stewart@schefferandrew.com
Subject: 7260100

Thank you for providing Alberta Tourism, Parks, Recreation and Culture with information regarding the Area Structure Plan within sections 29, 31 & 32-50-24-W4M. There are no significant previously recorded archaeological sites in these sections and as this area appears to be primarily disturbed through cultivation, it has low potential to contain intact archaeological sites. Areas of higher potential would include any areas with native vegetation. However, the Historic Resources Management branch recommends that Heritage Survey Site forms be completed for any other standing structures thought to be older than forty years.

Reporting the discovery of historical resources: Pursuant to Section 31 of the Historical Resources Act, should any historical resources be encountered during any activities associated with land disturbance operations, the Historic Resources Management Branch must be contacted immediately. It will then be necessary for to issue further instructions regarding the documentation of these resources. If you have any questions regarding the above, please do not hesitate to contact me.

Margret Ingibergsson

Historic Resources Management Branch
Alberta Tourism, Parks, Recreation and Culture
Old St. Stephen's College, 8820 - 112 Street
Edmonton, Alberta T6G 2P8
Phone: (780) 431-2374 / Fax: (780) 422-3106
Email: margret.ingibergsson@gov.ab.ca

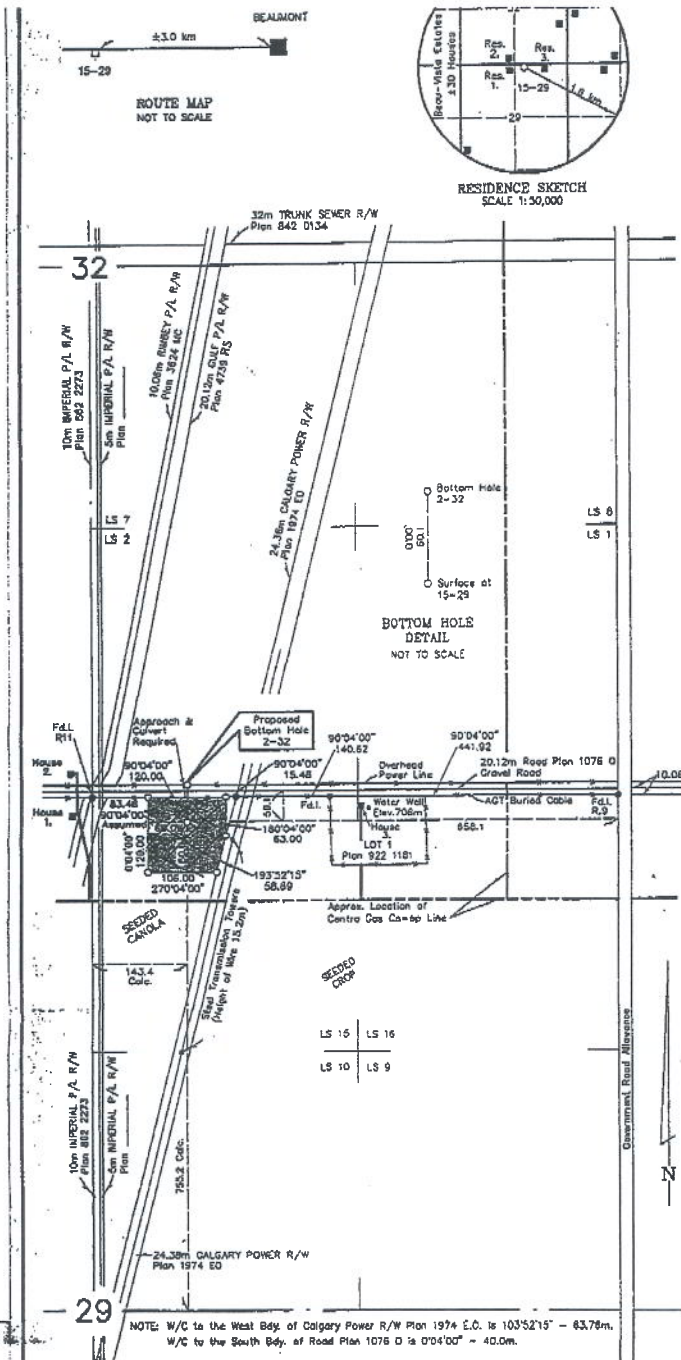
5/13/2008

East Vistas Area Structure Plan

Appendix B

Reclamation certificates for abandoned wells





NOTE: W/C to the West Bdy. of Calgary Power R/W Plan 1974 E.C. is 103'52'15" - 83.78m.
W/C to the South Bdy. of Road Plan 1076 D is 0'04'00" - 40.0m.

NOTE: THIS WELL IS TO BE DIRECTIONALLY DRILLED

RIFE NISKU 2-32-50-24

Well Site and Access Road

L.S. 15 Sec. 29 Twp. 50 Rge. 24 W. 4 M.

Scale 1:5000

BOTTOM HOLE 10.0 N. of S. } Bay. Sec. 32
CO-ORDS : 658.1 W. of E }

SURFACE 50.1 S. of N. } Bdy. Sec. 29
CO-ORDS : 658.1 W. of E. }

ELEVATIONS : 703.5 Ground

WELL SITE CORNERS:
N.W. 704.1 N.E. 704.1
S.W. 704.6 S.E. 705.4

AREAS:		ha	ac
WELL SITE	=	1.40	3.46
ACCESS ROAD	=	-	-
TOTAL	=	1.40	3.46

WELL LICENCING INFORMATION :
 THE PROPOSED WELL IS AT LEAST :
 - 1.5 km FROM THE CORPORATE LIMITS OF A CITY, TOWN OR VILLAGE
 - THE PROPOSED WELL IS OUTSIDE ANY POTENTIAL COAL DEVELOPMENT AREA
 - 400m FROM ANY SURFACE IMPROVEMENTS A 400m FROM ANY SURVEYED ROAD
 - 5.0 km FROM A LIMITED AERODROME
 - 1.8 km FROM AN UNLIGHTED AERODROME
 - NEAREST RESIDENCE IS 275'22" - 174m FROM WELL CENTER
 - NEAREST URBAN CENTRE IS BEAULHOUT EAST ± 3.0 km.

YES NO

10

RIFE RESOURCES LTD.

LEGEND:
Survey monuments found shown thus:
Survey monuments placed shown thus:
Portions referred to shaded thus:
Distances and elevations are in metres.

I certify that the survey represented by this plan is correct and true to the best of my knowledge and was completed on the 8th day of July, 1924.

NORTHCAN SURVEYS LTD.

ALLIANCE DOW DIRECTOR: Helen A. Ruch

Mr. Decker

Address _____

REVISIONS: 24/08/13—Original Su
04/07/07—1.1 to 1.5

94/07/07=Added Ed

NR - 944100



ENVIRONMENT
Finance, Administration and
Land Reclamation Services
Land Conservation and
Reclamation Council

Office of J.M. King, Chairman

3rd Floor, Oxbridge Place, 9820 - 106 Street, Edmonton, Alberta, Canada T6K 2J6 403/427-6202 Fax: 403/422-8233

November 7, 1991

County No. 25


RECLAMATION CERTIFICATE NO. 10483

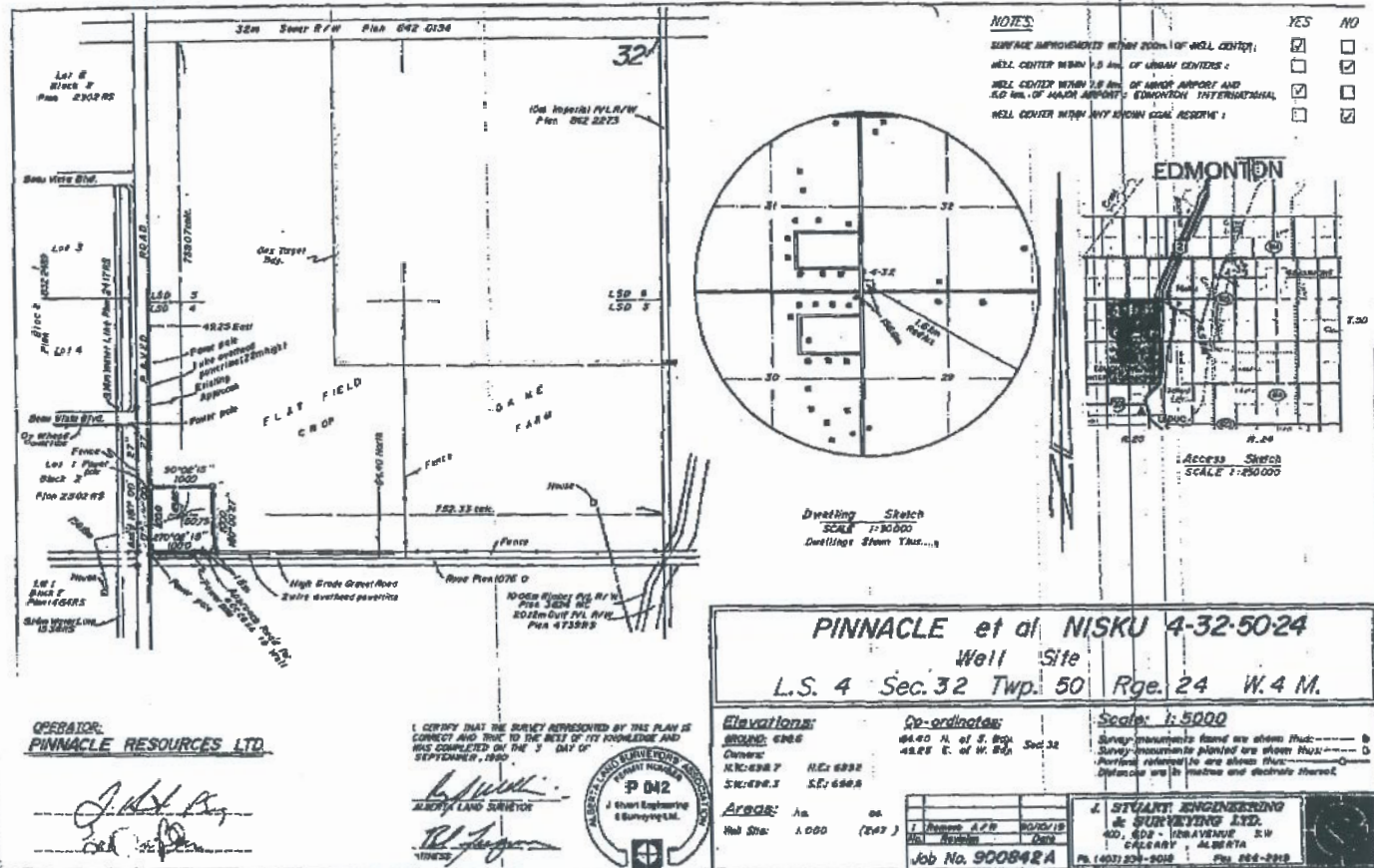
Pursuant to Section 55 subsection (1) of the Land Surface Conservation and Reclamation Act this is to certify that consent has been given to the surrender of the surface lease held by Pinnacle Resources Ltd.

within SW1/4 Sec. 32 Tp. 50 Rge. 24 W. 4th Mer.

in connection with or incidental to Pinnacle et al Nisku 4-32-50-24 well, as shown outlined in yellow on the plan attached hereto.

Pursuant to Section 55 subsection (2) of the Act the Council will be holding an inquiry with respect to the condition of the land referred to in this certificate.


Acting CHAIRMAN, LAND CONSERVATION
AND RECLAMATION COUNCIL





4th Floor, Oxbridge Place
9820 - 106 Street
Edmonton, Alberta
Canada T5K 2J6

Telephone (780) 427-5883
Fax (780) 422-4192

RECLAMATION CERTIFICATE NO. 39806
Well License Number 168314

This reclamation certificate is issued pursuant to section 123 of the Environmental Protection and Enhancement Act, following an inquiry on

May 30, 2000 (Date)

This certifies that the surface of the land held by Rife Resources Ltd.
within NE Sec. 29 Tp. 50 Rge. 24 W4M

in connection with or incidental to Rife Nisku 2-32-50-24 well, drilled from a surface point in
Lsd 15 of Sec. 29 Tp. 50 Rge. 24 W4M, as shown outlined in yellow on the attached plan,
complies with the conservation and reclamation requirements of Part 5 of the Act.

Issued this 30th day of May, 19 2000

Bio P. P. P.
Inspector (s)

Operator/Agent:

Rife Resources Ltd.
400 - 144 4TH AVE SW
CALGARY, AB
T2P 3N4

Owners/Occupants:

Frank Johnsen
Ann Johnsen

Section 84 of the Environmental Protection and Enhancement Act may provide a right of appeal against this decision to the Chair, Environmental Appeal Board. There may be a strict time limit for filing such an appeal. For further information, contact the Board Secretary of the Environmental Appeal Board at #400, Alberta Treasury Branches Plaza, 9925 109 Street, Edmonton, Alberta T5K 2J8; telephone (780)427-6207; fax (780)427-4693

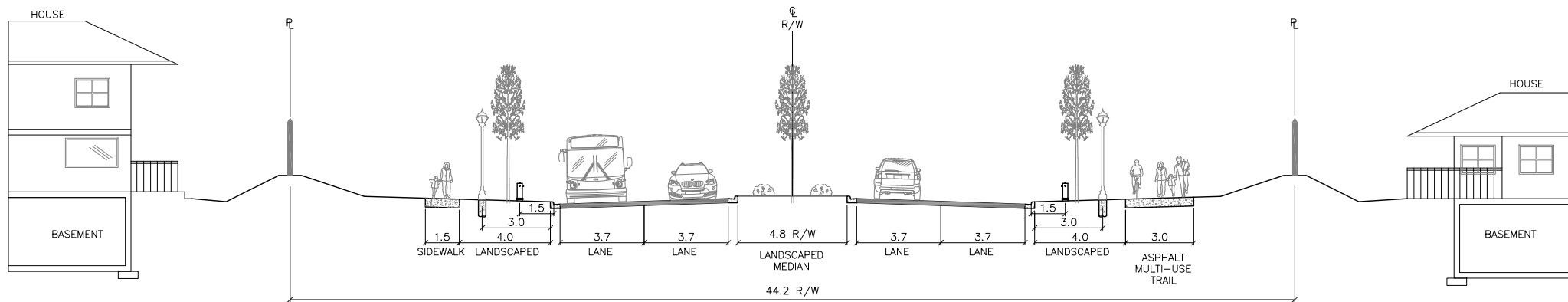
TERM OR CONDITION ATTACHED YES ☒ NO

East Vistas Area Structure Plan

Appendix C

Road cross-section examples

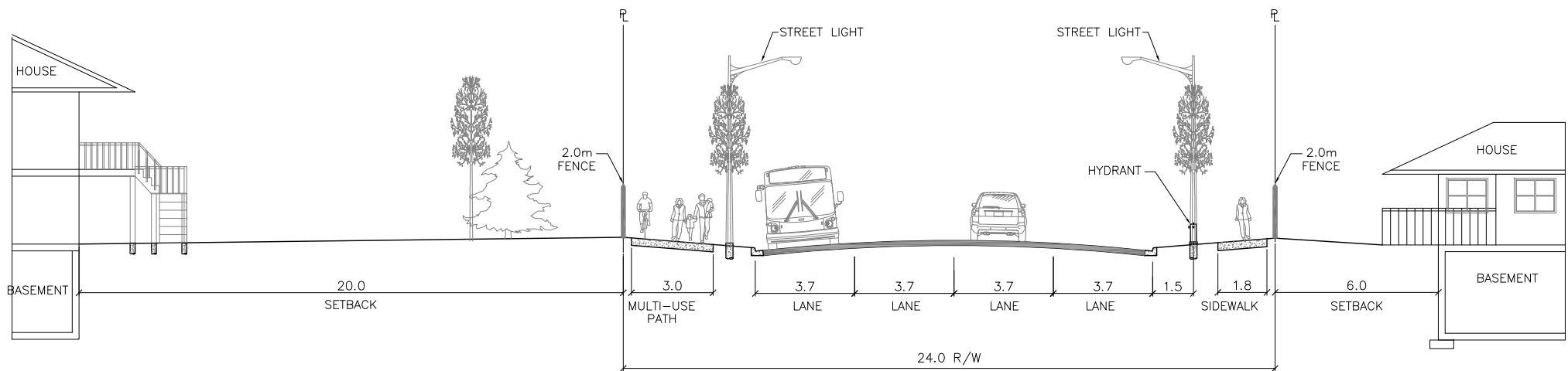




44.2m TYPICAL DIVIDED ARTERIAL

SCALE: NTS

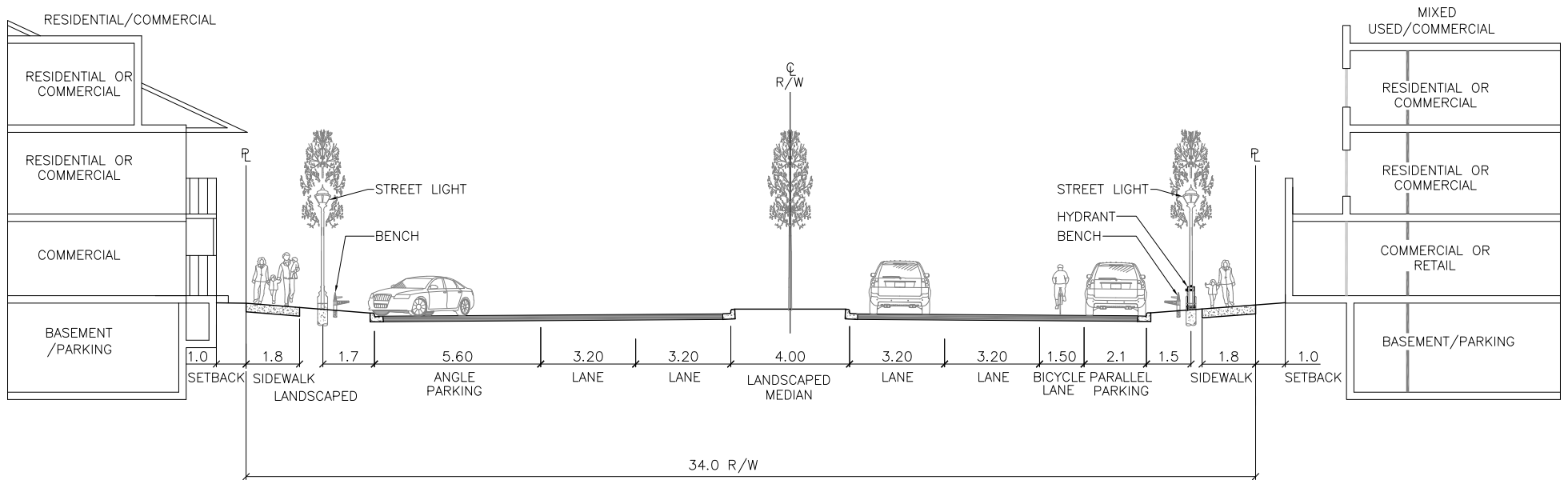
NOTE: ALL DISTANCES ARE IN METERS



24.0m TYPICAL MINOR ARTERIAL

SCALE: NTS

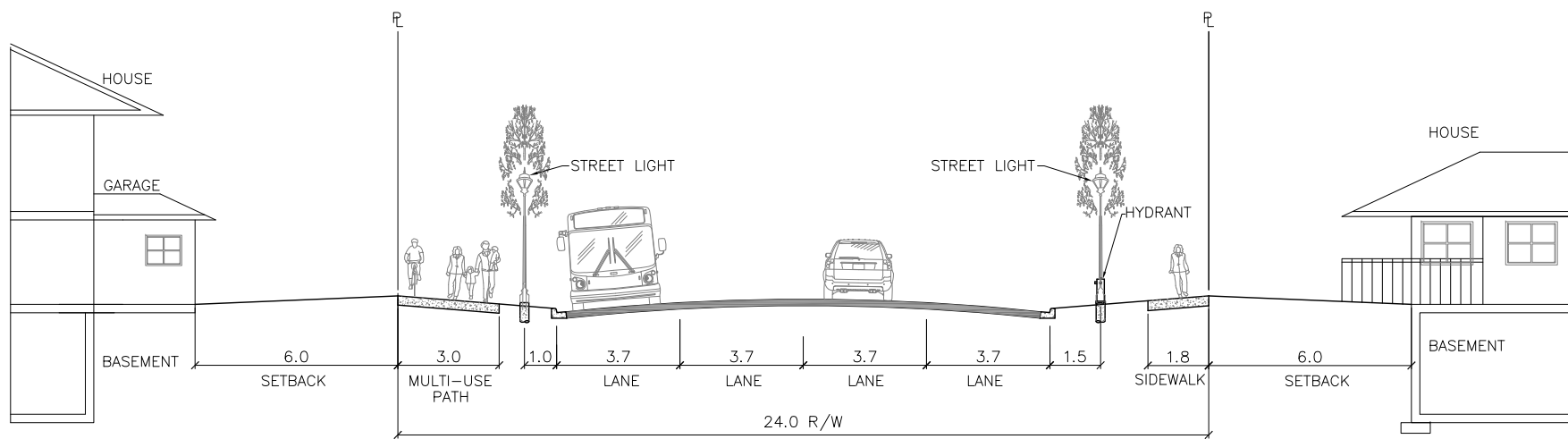
NOTE: ALL DISTANCES ARE IN METERS



34.0m TYPICAL MAIN STREET

SCALE: NTS

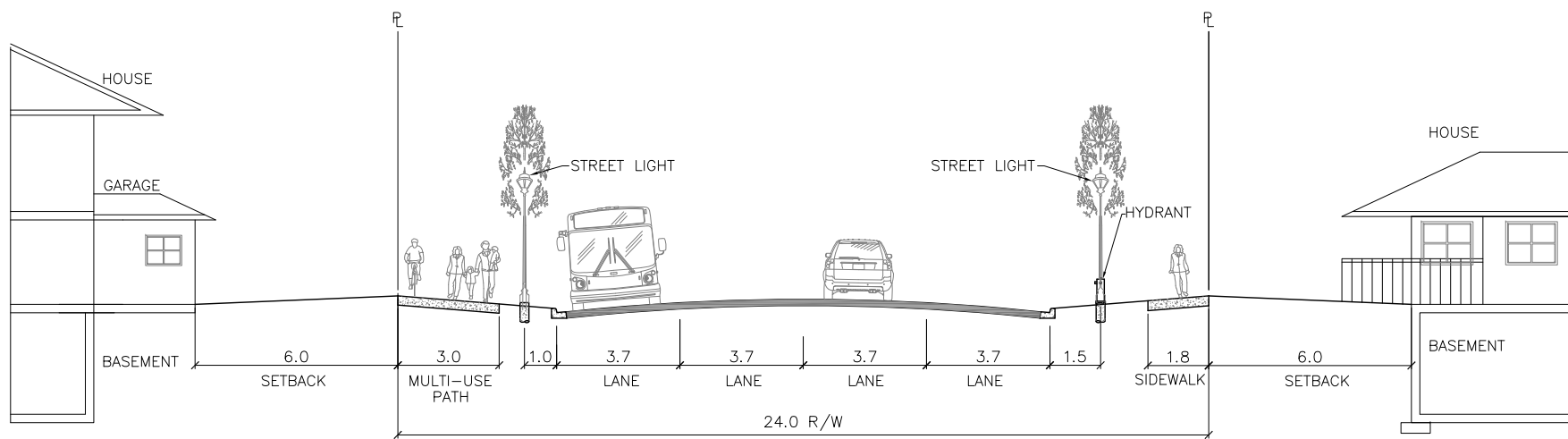
NOTE: ALL DISTANCES ARE IN METERS



24.0m TYPICAL RESIDENTIAL COLLECTOR

SCALE: NTS

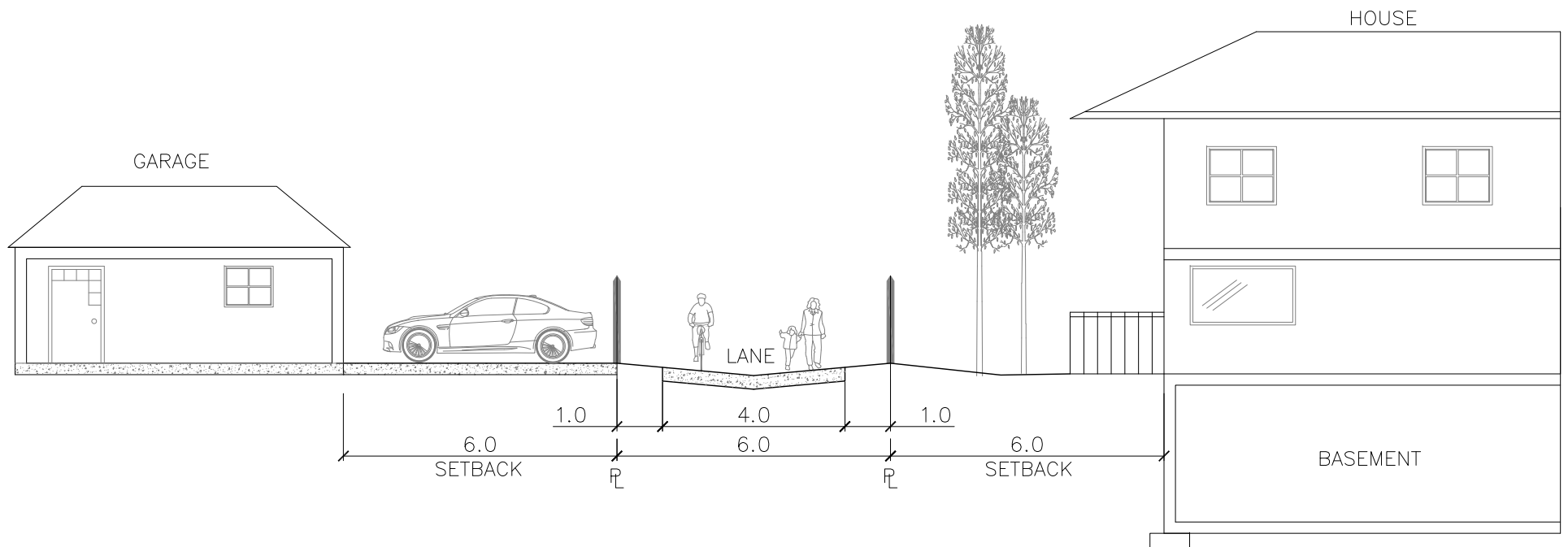
NOTE: ALL DISTANCES ARE IN METERS



24.0m TYPICAL RESIDENTIAL COLLECTOR

SCALE: NTS

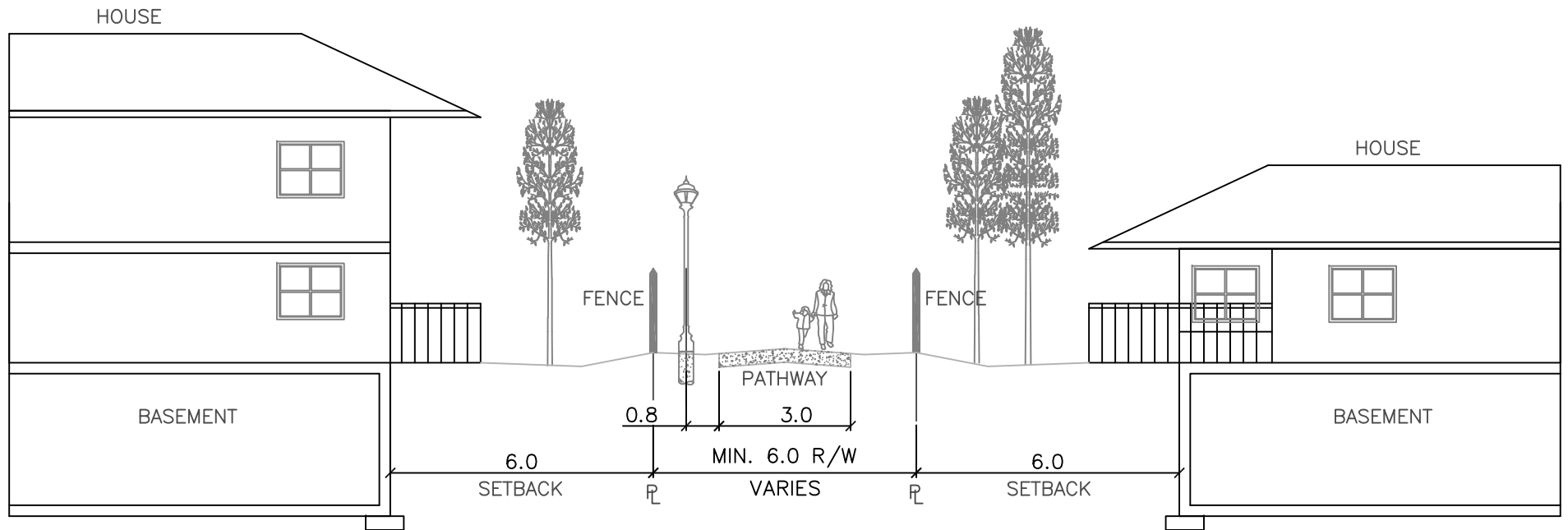
NOTE: ALL DISTANCES ARE IN METERS



6.0m TYPICAL RESIDENTIAL LOCAL ALLEY PAVED

SCALE: NTS

NOTE: ALL DISTANCES ARE IN METERS



6.0m TYPICAL RESIDENTIAL MULTI USE PATH

SCALE: NTS

NOTE: ALL DISTANCES ARE IN METERS

East Vistas Area Structure Plan

Appendix D

Open house questionnaire summaries





EAST VISTAS AREA STRUCTURE PLAN

JANUARY 16 2008 OPEN HOUSE

SUMMARY OF COMMENTS AND RESPONSES

Number of persons in attendance (according to the sign-in sheet): 62

1: The development concept shows an appropriate future land use scenario:

Agree	Neither Agree or Disagree	Disagree
20	2	5

Reasons for disagreeing:

- There is no mixed land use, should be a variety to accommodate affordability
- There is no provision for improved roadways – current roads are inadequate
- This will make the area a part of “the city”

2: There is an appropriate distribution of residential uses:

Agree	Neither Agree or Disagree	Disagree
22	1	4

Reasons for disagreeing:

- There is no mixed land use, should be a variety to accommodate affordability
- Doesn't like commercial area, townhouse development, etc.

3: There is an appropriate proportion of residential densities:

Agree	Neither Agree or Disagree	Disagree
18	3	7

Reasons for disagreeing:

- Would like to see a greater ranging including high density
- More variety needed, rather than three types of residential land use
- There should be a townhouse site in the SW corner
- Not enough roads for the number of homes proposed

- Too dense – 1 or 2 acre lots more appropriate for the area

4: This is an appropriate distribution of commercial sites:

Agree	Neither Agree or Disagree	Disagree
16	3	8

Reasons for disagreeing:

- Too focused on the east, west needs more commercial for balance
- There should be a commercial site in the SW corner
- None needed – Nisku/Beaumont/Leduc are close by
- Main street commercial should begin at RR 245 and go to RR 244 (mentioned twice)
- Main street should be along TWP 505

5: Development guidelines would be desirable in each neighbourhood:

Agree	Neither Agree or Disagree	Disagree
24	1	2

Reasons for disagreeing:

- Too much uniformity in appearance
- Broad guidelines are okay, architectural guidelines should be up to the developer

6: Trails and greenways, linking parks and recreational area are important:

Agree	Neither Agree or Disagree	Disagree
25	2	0

Reasons for disagreeing:

- There should be more

7: Please indicate which one of the following most closely applies to you:

Resident landowner within the ASP area	13
Non-resident landowner within the ASP area	1
Developer/Consultant representing lands in the ASP area	3
Local resident outside of ASP area	7
Did not respond	2
Other	3

8: Additional comments regarding the concept

Residential

- Higher standard of building and structural plans is needed.
- Should consider including retirement or assisted living housing.
- Should be at least four lots per acre.

Recreational

- Horse trails are needed.
- Preference for no ATVs within the plan area.

Environmental

- Please consider the control on garbage during construction. Temporary fence will be needed around construction sites. Developers should be held accountable for wayward garbage blown onto other properties.
- Concern over Blackmud Creek and spring flooding west of the ASP (mentioned twice).
- Wildlife will disappear.

Transportation

- Assurance is needed that if TWP 510 is upgraded access to properties that use it is maintained.
- Need better access to main arteries (HWY 625 and TWP 510) before the ASP is approved.
- Lights are needed on RR 245 and HWY 625.
- Concern over increased traffic and roadway capacity (mentioned twice).
- Street detail should be provided.

- Development will help to improve road conditions.

Municipal Services

- Sewage should be extended to the Vistas and Valley View estates.

General Concept

- Concern over how Beaumont views this ASP.
- Land between ASP and Beaumont should have been included (mentioned twice).
- This is a very preliminary concept in need of more detail.
- 50 metre setback from HWY 625 is a concern for land owners (loss of developable land).
- Town centre is in a good location.
- Bringing "city life" to the country is a bad concept.
- Crime and noise will increase.
- Streetlights and sidewalks are not for the "country."
- Mix of densities is a good idea.
- Should extend the main street concept to include RR 245 to RR 244.
- Very good use of land.

**EAST VISTAS AREA STRUCTURE PLAN
SECOND OPEN HOUSE – SEPTEMBER 22, 2009
SUMMARY OF COMMENTS AND RESPONSES**

Number of persons in attendance (according to the sign-in sheet): 82

1: The development concept shows an appropriate future land use scenario:

Agree	Neither Agree or Disagree	Disagree
7	2	3

Reasons for disagreeing:

- We moved to the North Vistas 19 years ago and have enjoyed establishing our quiet home in the country.

2: There is an appropriate distribution of residential uses:

Agree	Neither Agree or Disagree	Disagree
7	1	5

Reasons for disagreeing:

- Land is too high quality.
- Too much higher density residential & multi

3: There is an appropriate proportion of residential densities:

Agree	Neither Agree or Disagree	Disagree
7	3	2

Reasons for disagreeing:

- Could be more density.

4: This is an appropriate distribution of commercial sites:

Agree	Neither Agree or Disagree	Disagree
8	1	4

Reasons for disagreeing:

- Should be more commercial.

- Should be less commercial; use towns that are there.

5: Development guidelines would be desirable in each neighbourhood:

Agree	Neither Agree or Disagree	Disagree
8	1	2

Reasons for disagreeing:

- What guidelines for low density? – apartment looking houses like Lucas Estates.

6: Trails and greenways, linking parks and recreational area are important:

Agree	Neither Agree or Disagree	Disagree
13		

Reasons for disagreeing:

7: Please indicate which one of the following most closely applies to you:

Resident landowner within the ASP area	5
Non-resident landowner within the ASP area	
Developer/Consultant representing lands in the ASP area	1
Local resident outside of ASP area	8
Did not respond	
Other	1

- -Resident, but non-landowner.

8: Additional comments regarding the concept
Residential

-

Recreational

- The inclusion of walking pathways is excellent for the health of Leduc residents. I do hope that connection of this pathway system to Beaumont will take place in the future. Additionally, the county has a wonderful opportunity to connect into the Waskahagan Trail System. I do hope they capitalize on this opportunity for future generations.

Environmental

-

Transportation

- There are not enough roads to service the area. The 625 highway is already overloaded – new roads are needed to be developed and a berm built on the edge of highway 625.
- A decision of highway 625 future plans needs to be resolved sooner; as it affects our land directly. Please provide more information on this topic.
- Traffic assessment doesn't consider traffic flows to anticipate development east to Beaumont.
- That the roadways are upgraded in the following manner due to the increase in traffic:
 - a) Range Road 245 would need to be widened, with left/right turning lanes added.
 - b) At the intersection of Range Road 245 and Secondary Hwy 625, traffic lights would need to be installed. It is difficult enough now to turn left onto the highway from this range road during rush hour traffic from Nisku. Having higher traffic volume would only make this worse and dangerous. This might also be a factor for Range Road 244, as well as coming out of the South Vistas' onto the highway.
 - c) We constantly have problems with vehicles speeding down Range Road 245 as commuters use this road as a shortcut between Nisku and Edmonton, using 101 Street to go into the City. I trust the speed limit on the Range Road would not be increasing, and the posted speed of 60km would be enforced. Crosswalks would also be appreciated at the entrance of North Vistas loops crossing over to the East Vistas.
Not only trying to ensure that traffic flows safely, but that my kids can continue to walk and bike safely in the area.
- The allowance of future LRT down Township 510 demonstrates true long term vision. It will spare our children from the insane costs incurred by Edmonton in its University south LRT expansion. To have to remove homes due to lack of long term planning is deplorable. I am most pleased that the county is thinking this far in advance. This is clearly evidence of the county's capabilities and will serve the county well in any future annexation dispute.
- We would like to recommend the ring road to be named 'Gobeil Drive' in recognition of pioneering farmers in this area to developed.

Municipal Services

- ASP should have extended east to Beaumont to ensure orderly planning & adequate utilities.
- My concerns are that there is not a strain put on existing residents as far as utilities (water pressure, etc.)

General Concept

- Good plan.
- We feel like our quality of life will be impacted negatively. There will be increase in noise, light, activity, and traffic. It's long time residents, our family feel very saddened and disappointed.

- I believe this ASP should be extended straight north to complete the logical block of land up to 41 Avenue SW; so that north & south roads to Edmonton can be planned in a rational progression.
- Hurry up!
- It is a great plan. I think Leduc County should move faster to get it done, because I am hearing all of this for the last 3 years. I think before we hit another boom, something should happen here so people can buy houses now, when it's a little affordable.
- I would like to express support for the East Vistas plan. As a neighbour I believe there is a need some sort of smart growth, which I am sure this is the one.
- This would bring the value of homes in North Vistas down as there won't be that open feeling. One reason we couldn't sub-divide was increase in traffic. My 2.5 acres is not good agricultural land – now just growing weeds.
Why are wasting good agricultural land. Hasn't there been enough development around the Beaumont area with plenty of vacant lots. Go there and start a mini town.
- The 3 smaller parcels west of Lucas Phase I should be allowed to provide additional commercial space, I was under the impression that Tom Berube's parcel was already zoned commercial. This combined with the 2 adjoining small parcels could assist in transition area as well as provide opportunity for services for the early movers into the area.
- I guess this plan shows the smart growth – very impressed.
- More lots are important; more quality roads – not poor roads; more variety in size is important; the high density is concerning more schools proper road tax base to support this; I would disagree just lots lots with roads and good roads.

EAST VISTAS AREA STRUCTURE PLAN THIRD

OPEN HOUSE – FEBRUARY 24, 2010

SUMMARY OF COMMENTS AND RESPONSES

Number of persons in attendance (according to the sign-in sheet): 43

Email letter submissions: 1

1: The development concept shows an appropriate future land use scenario:

Agree	Neither Agree or Disagree	Disagree
6	2	5

Additional comments (paraphrased):

- Although the plan meets the guidelines of the Capital Region Plan, It is not an appropriate use of agricultural land.
- Definitely not.
- The land should be for agricultural use, not development because of the soil classification.
- Farmland and agriculture should take precedence.
- The larger regional plan is unknown at this time.
-

2: There is an appropriate distribution of residential uses:

Agree	Neither Agree or Disagree	Disagree
6	5	1

Additional comments (paraphrased):

- It would follow the Capital Region Plan's guidelines for residential density.

3: There is an appropriate proportion of residential densities:

Agree	Neither Agree or Disagree	Disagree
6	3	2

No additional comments

4: This is an appropriate distribution of commercial sites:

Agree	Neither Agree or Disagree	Disagree
5	4	3

Additional comments (paraphrased):

- Too many commercial sites would increase traffic.
- Unsure of the larger Edmonton-Beaumont-Leduc-Airport regional plan.

5: Development guidelines would be desirable in each neighbourhood:

Agree	Neither Agree or Disagree	Disagree
9	2	1

No additional comments

6: Trails and greenways, linking parks and recreational area are important:

Agree	Neither Agree or Disagree	Disagree
11	1	1

No additional comments

7: Please indicate which one of the following most closely applies to you:

Resident landowner within the ASP area	5
Non-resident landowner within the ASP area	0
Developer/Consultant representing lands in the ASP area	1
Local resident outside of ASP area	6
Other	0

8: Additional comments regarding the concept
Adjacent Municipalities and Lands

- Should include the sections (33 and 28) between the plan area and Beaumont.
- Adjacent landowners between the plan area and Beaumont should be approached to participate.

- This plan goes along with the Capital Region plan, but it does not follow the North Saskatchewan Regional Plan.
- The plan states that development is not to be permitted west of 814 until 2/3 of the east side is developed.
- The plan states that development is not to occur between Highways 814 and 2.
- Concerned about effects on developing a high density residential community close to an expanding industrial area.
- The Vistas were developed originally before Nisku expanded to the east.
- The Beaumont and District Ag Society being close to this development will create conflict with truck traffic and smells.
- Meets the guidelines of the Capital Region plan but there should be a compatibility study to determine if this community belongs here.
- Unsure of government involvement and opinions in this process (provincial and municipal).
- Would like to see the findings of the Leduc County/Town of Beaumont sustainability study be incorporated into the plan when they are available.
- Quiet lifestyle of neighbouring residents will be threatened by this development.
- Feeling is that the County's mind is made up on this development application.
- This development should be done as a 'regional structure plan', taking into consideration Edmonton, Leduc County, Leduc, Beaumont and the airport.
- Potential conflicts regarding servicing for Beaumont and southeast Edmonton.

Residential

- Developing lower income housing under larger power liens is unethical and planning without a conscience.

Residential

- Developing lower income housing under larger power liens is unethical and planning without a conscience.
- Lot sizes are too small; these lots will not provide enough backyard plant-life to filter air and noise pollutions.

Transportation

- Uncertainly as to how much land will be taken within and around the plan area to meet transportation demands for new and expanding roads.

Municipal Services

- Community does not have the immediate infrastructure to sustain it, and it will negatively affect Beaumont and its services.
- Uncertainty if the waterline will be installed under the highway or within the plan area along highway 625.

General Concept

- Land should not be covered with solid concrete.

- This concept is too close to other developments in the City of Edmonton, City of Leduc and the Town of Beaumont
- Recommendations to rename streets, parks and other features in memory of the Gobeil (unsure of spelling) family who have lived on these lands for many decades.
- Good concept.
- This concept is not in the best interest of the area.
- This development should be on the west side of Highway 2 near Devon.